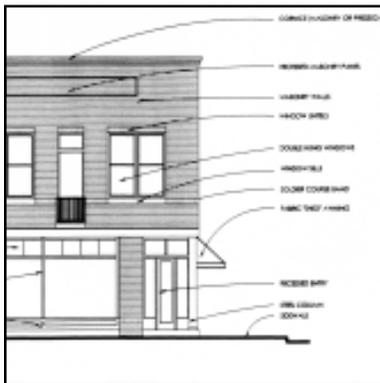


# Clayton

## Downtown Master Plan

October 2010

updated from August 20, 2001



Prepared For:

**Town of Clayton  
and  
The Downtown Development Association**

Prepared By:



306/200 Parham Street  
Suite B  
Raleigh, NC 27601  
919.755.0046



600 Wade Avenue  
Raleigh, NC 27605  
919.832.2878

November 17, 2010

**Town of Clayton**  
**Clayton Downtown Development Association**

**Attention:** *Mr. Jody Mcleod, Mayor*  
*Mr. Steve Biggs, Town Manager*

*Mr. Ed Knight, President*  
*Downtown Development Association*

*Mr. Bruce Naegelen*  
*Downtown Development Coordinator*

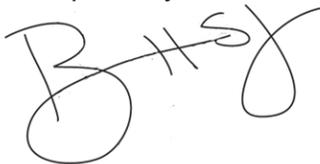
**Reference:** *Clayton Downtown Master Plan*

Gentlemen,

obs landscape architects in association with Winstead Wilkinson Architects is pleased to provide you this updated Master Plan for Downtown Clayton. We appreciate the opportunity to assist the town with this update of the plan originally prepared in 2001.

Downtown has made significant strides over the past 10 years and it is our hope that this document will continue to provide practical guidance for continued development in downtown, enhancing the quality of life for the town's citizens as well as visitors.

Respectfully submitted



Brian H. Starkey, RLA / ASLA  
Principal

DOWNTOWN IMPROVEMENTS PROJECT



Prepared For:  
THE TOWN OF CLAYTON

DOWNTOWN DEVELOPMENT ASSOCIATION



landscape architects  
land planners

306/200 Parham Street  
Suite B  
Raleigh, NC 27601  
919.755.0046



WINSTEAD WILKINSON

Architects, PLLC

600 Wade Avenue  
Raleigh, NC 27605  
919.832.2878

Panel 1 of 4

NO.	DESCRIPTION	DATE
1	Town Comments	10/11/10

MASTER PLAN

Date Issued: 8.06.01  
Scale: 1"=60'  
Drawn by: SHC  
Checked by: BHS  
Sheet No.

LA-1



MASTER PLAN



LEGEND



EXISTING BUILDING: USE TO REMAIN

THIS DESIGNATION REFERS TO A STRUCTURE WHICH IS CURRENTLY HOME TO A VIABLE AND CONTRIBUTING BUSINESS OR A RESIDENT. MANY OF THESE BUILDINGS ALSO HAVE POTENTIAL FOR ADAPTIVE REUSE.



EXISTING BUILDING: ADAPTIVE REUSE

THIS DESIGNATION REFERS TO A STRUCTURE WHICH IS CURRENTLY VACANT AND/OR TO HAVE SIGNIFICANT POTENTIAL FOR ADAPTIVE REUSE. MANY OF THESE STRUCTURES ARE HISTORIC IN CHARACTER.



PROPOSED BUILDING: HOUSING

THIS DESIGNATION REFERS TO STRUCTURES WHICH ARE PRIMARILY RESIDENTIAL.



PROPOSED BUILDING: MULTI - USE

THIS DESIGNATION REFERS TO PROPOSED STRUCTURES WHICH COULD BE COMMERCIAL AND/OR OFFICE USE. THEY COULD ALSO INCLUDE RESIDENTIAL.

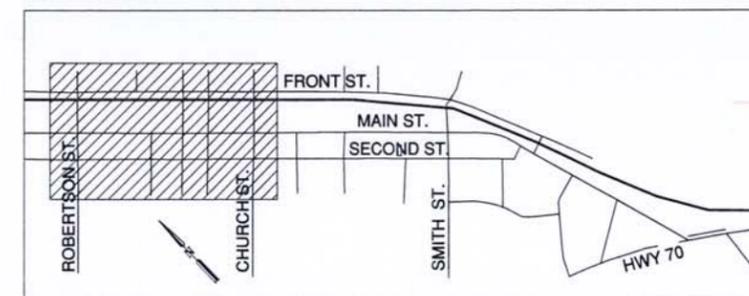


HISTORIC DISTRICT BOUNDARY



RAILROAD RIGHT OF WAY

KEY MAP



**DOWNTOWN IMPROVEMENTS PROJECT**



Prepared For:  
**THE TOWN OF CLAYTON**

**DOWNTOWN DEVELOPMENT ASSOCIATION**



306/200 Parham Street  
Suite B  
Raleigh, NC 27601  
919.755.0046



600 Wade Avenue  
Raleigh, NC 27605  
919.832.2878

Panel 2 of 4

REVISIONS:

NO.	DESCRIPTION	DATE
1	Town Comments	10/11/10

**MASTER PLAN**

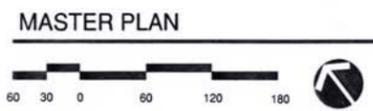
Date Issued: 8.06.01  
1"=60'  
Drawn by: SHC  
Checked by: BHS  
Sheet No.

**LA-2**

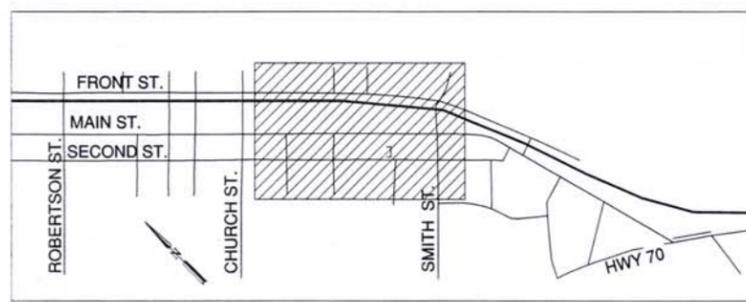


**LEGEND**

- EXISTING BUILDING: USE TO REMAIN**  
THIS DESIGNATION REFERS TO A STRUCTURE WHICH IS CURRENTLY HOME TO A VIABLE AND CONTRIBUTING BUSINESS OR A RESIDENT. MANY OF THESE BUILDINGS ALSO HAVE POTENTIAL FOR ADAPTIVE REUSE.
- EXISTING BUILDING: ADAPTIVE REUSE**  
THIS DESIGNATION REFERS TO A STRUCTURE WHICH IS CURRENTLY VACANT AND/OR TO HAVE SIGNIFICANT POTENTIAL FOR ADAPTIVE REUSE. MANY OF THESE STRUCTURES ARE HISTORIC IN CHARACTER.
- PROPOSED BUILDING: HOUSING**  
THIS DESIGNATION REFERS TO STRUCTURES WHICH ARE PRIMARILY RESIDENTIAL.
- PROPOSED BUILDING: MULTI - USE**  
THIS DESIGNATION REFERS TO PROPOSED STRUCTURES WHICH COULD BE COMMERCIAL AND/OR OFFICE USE. THEY COULD ALSO INCLUDE RESIDENTIAL.
- HISTORIC DISTRICT BOUNDARY**
- RAILROAD RIGHT OF WAY**



**KEY MAP**





Front Street corridor is subject to future study

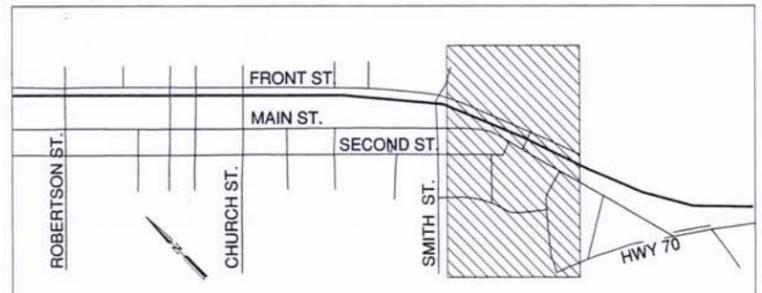
**LEGEND**

- 
**EXISTING BUILDING: USE TO REMAIN**  
 THIS DESIGNATION REFERS TO A STRUCTURE WHICH IS CURRENTLY HOME TO A VIABLE AND CONTRIBUTING BUSINESS OR A RESIDENT. MANY OF THESE BUILDINGS ALSO HAVE POTENTIAL FOR ADAPTIVE REUSE.
- 
**EXISTING BUILDING: ADAPTIVE REUSE**  
 THIS DESIGNATION REFERS TO A STRUCTURE WHICH IS CURRENTLY VACANT AND/OR TO HAVE SIGNIFICANT POTENTIAL FOR ADAPTIVE REUSE. MANY OF THESE STRUCTURES ARE HISTORIC IN CHARACTER.
- 
**PROPOSED BUILDING: HOUSING**  
 THIS DESIGNATION REFERS TO STRUCTURES WHICH ARE PRIMARILY RESIDENTIAL.
- 
**PROPOSED BUILDING: MULTI - USE**  
 THIS DESIGNATION REFERS TO PROPOSED STRUCTURES WHICH COULD BE COMMERCIAL AND/OR OFFICE USE. THEY COULD ALSO INCLUDE RESIDENTIAL.
- 
**HISTORIC DISTRICT BOUNDARY**
- 
**RAILROAD RIGHT OF WAY**

**MASTER PLAN**



**KEY MAP**



**DOWNTOWN IMPROVEMENTS PROJECT**



Prepared For:  
**THE TOWN OF CLAYTON**

**DOWNTOWN DEVELOPMENT ASSOCIATION**



landscape architects  
land planners  
306/200 Parham Street  
Suite B  
Raleigh, NC 27601  
919.755.0046



**WINSTEAD WILKINSON**  
Architects, PLLC  
600 Wade Avenue  
Raleigh, NC 27605  
919.832.2878

Panel 3 of 4

REVISIONS:

NO.	DESCRIPTION	DATE
1	Town Comments	10/11/10

**MASTER PLAN**

Date Issued: 8.06.01  
1"=60'  
Drawn by: SHC  
Checked by: BHS  
Sheet No.

**LA-3**



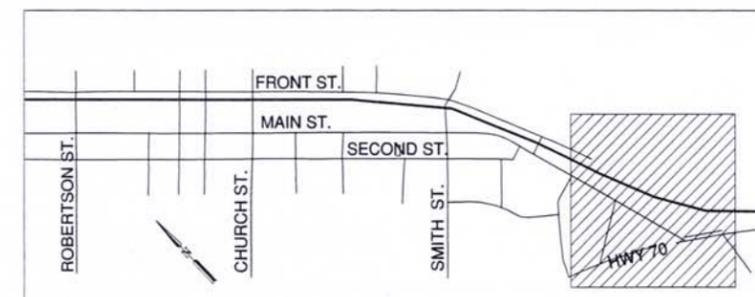
**LEGEND**

- EXISTING BUILDING: USE TO REMAIN**  
 THIS DESIGNATION REFERS TO A STRUCTURE WHICH IS CURRENTLY HOME TO A VIABLE AND CONTRIBUTING BUSINESS OR A RESIDENT. MANY OF THESE BUILDINGS ALSO HAVE POTENTIAL FOR ADAPTIVE REUSE.
- EXISTING BUILDING: ADAPTIVE REUSE**  
 THIS DESIGNATION REFERS TO A STRUCTURE WHICH IS CURRENTLY VACANT AND/OR TO HAVE SIGNIFICANT POTENTIAL FOR ADAPTIVE REUSE. MANY OF THESE STRUCTURES ARE HISTORIC IN CHARACTER.
- PROPOSED BUILDING: HOUSING**  
 THIS DESIGNATION REFERS TO STRUCTURES WHICH ARE PRIMARILY RESIDENTIAL.
- PROPOSED BUILDING: MULTI - USE**  
 THIS DESIGNATION REFERS TO PROPOSED STRUCTURES WHICH COULD BE COMMERCIAL AND/OR OFFICE USE. THEY COULD ALSO INCLUDE RESIDENTIAL.
- HISTORIC DISTRICT BOUNDARY**
- RAILROAD RIGHT OF WAY**

**MASTER PLAN**



**KEY MAP**



**DOWNTOWN IMPROVEMENTS PROJECT**



Prepared For:  
**THE TOWN OF CLAYTON**

**DOWNTOWN DEVELOPMENT ASSOCIATION**



landscape architects  
land planners  
306/200 Parham Street  
Suite B  
Raleigh, NC 27601  
919.755.0046



**WINSTEAD WILKINSON**  
Architects, PLLC  
600 Wade Avenue  
Raleigh, NC 27605  
919.832.2878

Panel 4 of 4

REVISIONS:

NO.	DESCRIPTION	DATE
1	Town Comments	10/11/10

**MASTER PLAN**

Date Issued: 8.06.01  
1"=60'  
Drawn by: SHC  
Checked by: BHS  
Sheet No.

**LA-4**

# Clayton

## Downtown Master Plan

Prepared For  
The Town of Clayton  
and  
The Downtown Development  
Association

**October 2010**  
**updated from August, 2001**



306/200 Parham Street  
Suite B  
Raleigh, NC 27601  
919.755.0046



600 Wade Avenue  
Raleigh, NC 27605  
919.832.2878

## **Table of Contents**

---

<b>Section I / Downtown Planning Guidelines</b>	<b>I-3</b>
Introduction	I-4
Inventory & Analysis	I-9
Planning Process	I-18
Master Plan	I-23
<b>Section II / Architectural &amp; Design Guidelines</b>	<b>II-1</b>
<b>Section III</b>	
Appendix A -- Workshop #1	A-2
Appendix B -- Workshop #2	A-14
Appendix C -- Maintenance Checklist	A-20
Appendix D -- Additional Resources / Publications	A-25
Appendix E -- Federal Tax Credit Summary	A-26
Appendix F -- Secretary of Interior Renovation Standards	A-29
Appendix G -- 2010 Workshop Materials	A-30

# Table of Contents / Section I

- 1. Introduction
  - Project Initiation / Purpose I-4
  - Strategic Growth Plan I-5
  - History I-6
  - Recent Projects I-8
  
- 2. Inventory & Analysis
  - General Observations I-9
  - Robertson St. to Church St. I-10
  - Church St. to Lombard St. I-12
  - Lombard St. to Smith St. I-14
  - Smith St. to US HWY 70 I-16
  - Railroad Corridor I-17
  
- 3. Master Plan
  - Design Guidelines
    - Development Opportunities I-18
    - Pedestrian Orientation I-19
    - Parking Areas I-20
    - Community Space I-21
    - Adjacent Properties I-22
    - Entries I-23
  
  - Plan Components
    - New Construction & Demolition I-24
    - Adaptive Reuse / Historic Preservation I-25
    - Streetscape Guidelines I-25
    - Railroad Corridor I-27
    - First St. Improvements I-28
    - Parking Areas / Structures I-28
    - Parking / Pedestrian Connections I-29
    - Community Spaces I-30
    - Gateway / Entry Areas I-31
  
  - Implementation I-37

## Project Initiation / Purpose

### Purpose

The purpose and intent of this plan is to direct the design of buildings and sites within the downtown commercial district (B1), in compliance with the Town's Unified Development Code and Strategic Growth Plan. The report is intended to promote a quality oriented downtown that serves as the social and cultural hub of Clayton. Buildings and sites should convey a sense of permanence, attention to detail, quality and investment.

The guidelines are intended to add consistency and predictability to the permit review process. Proposed developments within the Central Business District shall exhibit design elements that contribute to the unique sense of character of Downtown Clayton. This report also identifies public projects that will benefit the downtown as a whole, making it a more attractive and stable environment, and community center.



Main Street 1915



Main Street 1961

### Clayton / General Description

Clayton is a community of more than 15,000 citizens (2007), located 15 miles south of Raleigh in Johnston County. From 1990 the population of Clayton grew from 4,756 citizens to its current population. Johnston County's population is 168,525 (2009). Projections show Clayton's growth will continue at a similar pace into the future. Clayton is the largest municipality in Johnston County.

## Strategic Growth Plan

Like many towns, Clayton has sought an identity that will define it into the future, an identity that acknowledges the importance of the past and embraces a vision for the future setting Clayton apart as a desirable place to live, work, and play.

In 2001, the *Downtown Improvements Project* study was developed as part of a recommended goal of the Town's *2000 Strategic Growth Plan*. That document was created to help coordinate the development of planning documents and to provide a dynamic reference for decision makers in growth related matters.

Included in the goals identified by the plan is the development of downtown as a community hub. Since the 2001 Downtown Improvement Projects study has been the guiding document to achieve downtown goals. Additional tools have since been developed: *Strategic Economic Development Plan* (2004); Hiring of a full-time Downtown Develop Coordinator (2005); Restructure of the Downtown Development Association (2006); Acceptance into the NC Main Street Program (2006);



Main Street

*Clayton Downtown Parking Needs Assessment & Inventory* (2007); *NC Main Street Resource Team Report* (2007); *Market Analysis, Branding & Wayfinding Strategy* (2008); Listing of the Clayton Historic District on the National Register of Historic Places (2010).

In conjunction with *Downtown Clayton: Vision 2012* the DDA has developed ambitious work plans each year to accomplish goals tied to the vision. Accomplishments are measured each year in an Annual Report.



Corner of Main Street and Barbour Street

## History



Compton House

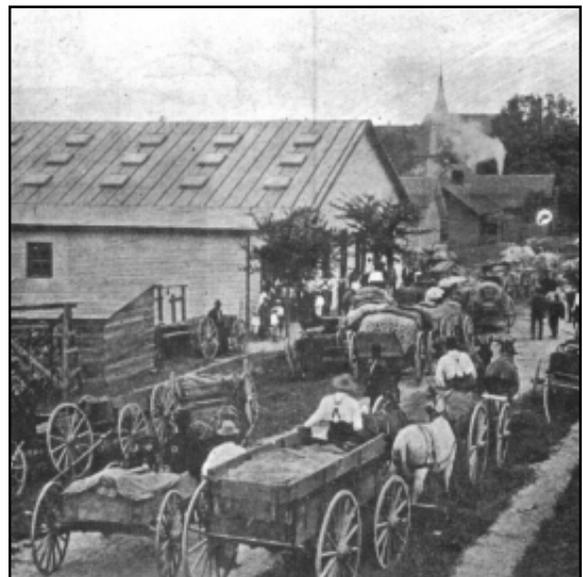
In 1853, the railroad came to town and the community was first known as Stallings' Station.

Mr. Ashley Horne developed a successful farming operation and merchandising business and became one of the most successful merchants and manufacturers in Clayton and in North Carolina. Horne's success inspired two other men, McCullers and Barbour, to open businesses that started an era of growth that lasted over 60 years. Businesses that flourished during that time were a turpentine distillery, lumber plants, a brick kiln, a cotton gin, gristmill, sawmill, tobacco warehouses and three cotton mills.

In the early 1900's Clayton was a major market for cotton, watermelon and tobacco and cited as one of the best cotton markets in the state. In 1907, the Raleigh Evening Times wrote that there was, "...more money per capita in Clayton than any city its size in the world."

Clayton experienced the loss of its financial eminence in the 1930's with the onset of the Great Depression, and Clayton's population grew slowly for forty years, increasing from 1,440 residents in 1910 to 2,229 in 1950. By the 1960's, cotton was out and tobacco was in, although not to the extent cotton had been, and the two mills eventually closed in the early 1960's. However, the population by now had grown to 3,312.

By the mid 1970's farming was beginning to phase out as an economic force in Clayton. The population had dropped slightly in 1970, to 3,103. An industrial base began taking shape in the county; residential growth increased in the Raleigh area and Clayton's workforce adapted, shifting to a more service/trades-oriented economy. In 1980 the population had increased to 4,091 and increased to 4,756 by 1990. Now, with a population of over 15,000 residents, manufacturers like Caterpillar, and bio-pharmaceutical companies like Talecris and Novo Nordisk employ many Clayton area residents, although many residents work out of town.



Tobacco market of 1909 or watermelon market of 1908.

## History

The Clayton Historic District comprises both the town's historic business district and some of its residential neighborhoods. Buildings within the district include two properties already on the National Register of Historic Places, the Clayton Banking Company Building, which is now the Clayton Chamber of Commerce and the Clayton Graded School and Auditorium, which is now known as The Clayton Center.

Several significant structures have been lost over the years, including the Horne residence and the Robertson Hotel. The commercial area of the historic district is densely developed and has many well-preserved, late nineteenth- and early-to-mid-twentieth-century commercial buildings with handsome storefronts and decorative cornices.

About 70% of the buildings within in the commercial section of the historic district are contributing structures and account for 22% of the entire historic district.

Many of these buildings can and have been adapted for new uses which echo the character of Clayton's history.



Horne residence



Existing building / adaptive reuse

Some of these are as follows;

- Clayton Oil Mill Building
- North Carolina Paper Company Building
- Old Town Hall Building
- Gulley Building Part of
- Various residential structures

The Clayton Historic District was listed on the National Register of Historic Places in June, 2010.



Spinning Mill / adaptive reuse

## Recent Projects

In recent years downtown Clayton has seen new public and private investment and development.

The old Clayton Elementary and Clayton High schools have been renovated into what is now The Clayton Center, which houses Town Hall and Cultural Arts Center.

The old school playground has been turned into the Town Square at W Main Street and Fayetteville Street

Town Lot at Lombard & E Main has been re-developed into a public plaza and parking

Police Department property is being redeveloped into the Clayton Law Enforcement Center with completion expected in 2011.

Recent private investment has tended toward renovation and reuse, including the examples on the next page.



113 E First Street - re-development



Old Town Hall



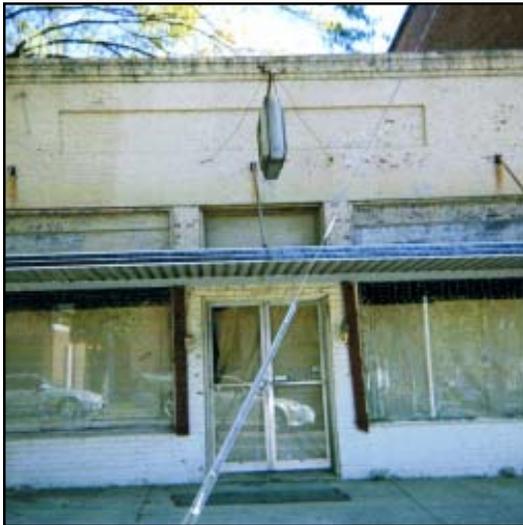
Clayton Elementary School / Clayton Center



131 E First Street - Before



113 E First Street - After



126 E First Street - Before



126 E First Street - After



104 N Fayettevill Street - Before



104 N Fayettevill Street - After

## General Observations

Downtown Clayton has retained a vitality and vibrancy in spite of sprawl and the development of retail corridors along thoroughfares. Evidence of this vitality is found in continued private and public investment, and a sincere interest throughout the community in improving downtown as well as committed town leadership.

## Study Area

The area examined by this study centers on Main Street from Robertson Street to the intersection of Main Street with US 70. The area extends to the east including the railroad corridor and to the west including Second Street. While the study considers the relationship of this area to US 70 and the rest of the Clayton area, the recommendations focus directly on the area as described.

## Organization

For purposes of the inventory and analysis the project area is subdivided into 4 sub areas. These areas are as follows:

- Robertson Street to Church Street
- Church Street to Lombard Street
- Lombard Street to Smith Street
- Smith Street to US 70

In addition to these areas the corridor associated with the railroad and its right of way is addressed separately.

Primary to the inventory and analysis are the following considerations:

- Land Use
- Buildings
- Circulation Patterns
- Surrounding land use



Corner of Main St. and Church St.

## Development Opportunities

There are many development opportunities available in downtown Clayton as shown on the Master Plan maps.

- LA-1 Robertson Street to Church Street
- LA-2 Church Street to Smith Street
- LA-3 Smith Street to Compton Street
- LA-4 Compton Street to US 70 Business

**Robertson St. to Church St.**

The corridor from Robertson Street to Church Street includes a diversity of land uses from warehousing and retail to residential and includes some office uses. This area also includes the Town Square, which is surrounded by Main, Second, O’Neil and Fayetteville Streets. Since 2001 this area has become Clayton Town Square, hosting concerts, festivals, and public gatherings.

Buildings of significance and interest include the North Carolina Paper Company building, the old train depot, the old elementary school, churches, Library and several buildings along First Street.

The western boundary of the Historic District within the commercial business district begins at O’Neil Street. Nearly every structure from O’Neil to Church Street and from First Street to Second are considered “contributing” structures. It is also in this area where buildings face First Street.



Town Square during the Farmer’s Market

The corridor between Robertson and O’Neil is not within the Historic District, but does contain several buildings of significance, including the old train depot building which has been relocated and turned into residential units; T.R. Lee gas station, and the old Clayton Oil Mills Building.

Few residential uses remain and these remaining homes are surrounded by incompatible land uses. Vacant property in combination with the removal of incompatible uses on adjacent properties offer opportunities for development.

Robertson and Fayetteville Streets are considered to be entry corridors to downtown Clayton and the neighborhoods east of the railroad tracks due to their strong connections to US 70.

Development opportunities can be found on Map LA-1.



NC Paper Co.

The development of Ellington Street is inappropriate and has left a clear example of how not to approach town redevelopment. The parking in front of the buildings with no buffer to the street and no sidewalks has resulted in a large asphalt parking area, which is a negative precedent.

A number of properties along Main Street also allow parking in front of buildings in areas not designed for that use. This activity is aesthetically distracting and also impedes pedestrian movement. Many of the existing sidewalks are not continuous.

First Street was historically vital to Clayton in the early 1900s. There are several brick buildings and a number of residential structures that have been redeveloped for commercial and office use.



Ellington Street



First Street buildings



First Street buildings

## Church St. to Lombard St.

The corridor from Church Street to past Lombard Street is considered to be the heart of downtown Clayton. In this area the buildings along Main Street are some of the most significant buildings in Clayton and a consistent relationship to the street is maintained with only a few exceptions. These exceptions include 208 E Main (convenience store) and the doctor's office across the street. Significant buildings include the Chamber of Commerce building, the Gully Building, 217-219 E Main; 218-220 E Main, and the building which houses the Coffee Mill. Other buildings, while not individually significant, together create a consistent block face.

Building uses are commercial and office in nature. The most significant undeveloped piece of land lies on the north side of Main Street at the intersection of Lombard Street, which has recently been developed as a public plaza and parking. This was the site of the Horne residence, a magnificent structure belonging to one of Clayton's early leaders, which was demolished in the early 1980's. Other vacant lots in the area between the railroad right-of-way and the buildings fronting Main Street offer infill development opportunities.



New public plaza/site of former Horne residence



Library Parking lot

Second Street in this area includes single family residential from Church Street to Barbour Street with Town Hall, old post office and Police Station anchoring the intersection at Barbour Street. Other land uses include the Clayton Square Shopping Center. Much of the land on the north side of Second Street, past Barbour St., is vacant or underutilized.

First Street (aka "Railroad Street") in this area is located entirely in the railroad right-of-way and remains unimproved. There are no buildings which front First Street or the railroad corridor.

Lombard Street is considered a primary access and a main entry to downtown due to its connection to US 70 and as an extension of NC 42 as well as its intersection with Main Street.



Unimproved First Street

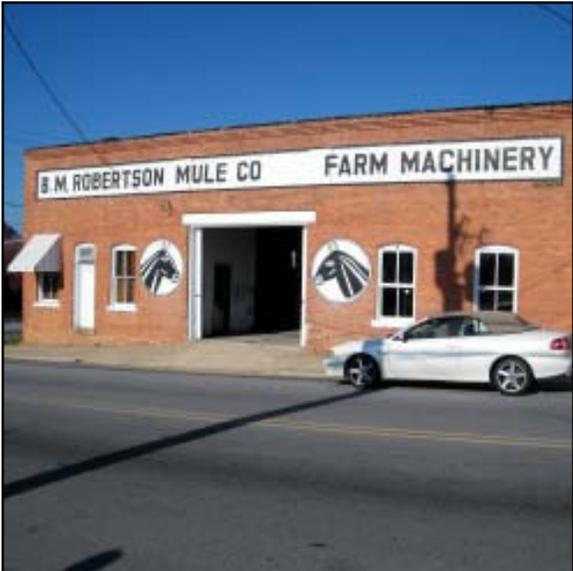
Lombard does not continue across Main Street as a street, but it is actually continued across as a driveway used to access the parking area and access to buildings between railroad right-of-way and Main Street.

Barbour Street also connects to US 70 but does not carry the traffic that Lombard Street does. The section of Barbour Street between Second and Main is one way and narrow in section.

Development opportunities can be found on map LA-2.



Coffee Mill



Mule Barn Building

## Lombard St. to Smith St.

Along Main Street and halfway between Lombard Street and Smith Street, the feeling of “downtown” starts to dissipate. This is due to the relationship of buildings to the street, the presence of residential structures and an increased variety of land uses. At this halfway point there exists a large vacant lot. From this halfway point to the intersection of Main Street and Smith Street most structures are residential in character. However, many of these structures are commercial in use or have been converted to commercial use.

While Smith Street serves as a connection from residential land uses to the west it is not considered an entry to downtown. The Smith Street railroad crossing was closed in 2006 as a safety project, eliminating its importance as an east-west connector.

On both sides of Second Street in this area land use is almost entirely residential in nature, characterized by single family residences.



Business at corner of Main Street and Smith Street



Former Smith Street railroad crossing

Also within this area there are a couple of significant structures. One of these is the Horne Building. This structure is situated very close to the Railroad tracks and is not visible from Main Street.

The area between the buildings fronting Main Street and the railroad tracks, with the removal of a few insignificant structures along Main Street, offers an opportunity for additional parking. In this section vehicles are being parked in front of buildings that are new and old, creating an undesirable visible condition along the street.

Development opportunities can be found on map LA-2.



Horne Building



Main Street

## Smith Street to US 70

The area from the intersection of Main Street and Smith Street to US 70 exhibits a variety of land uses culminating in a host of retail uses at the intersection with US 70.

From Smith Street to past the intersection of Main with Second Street the railroad parallels Main Street and there are no buildings between Main Street and the railroad with the exception of one residential structure near the intersection of Smith Street.

Land uses on the west side of Main Street includes single family residences, several commercial uses, a few vacant lots, and a multi-family development and retail at the intersection. First Street in this area does not exist.

The area on the east side of Main Street is narrow because of the railroad right of way and the predominant land has not been developed, with a few single family residences.

At the intersection of Second Street and Main Street is the site of the historic Compton house. This is the structure that was damaged during the retreat of Johnson's troops during the Civil War. While this house sits back from the street, it's presence is significant and integral to Clayton's history.

The intersection of Second Street itself is offset by approximately 150 feet from the intersection of Central Street, which crosses the railroad tracks and leads to the post office and the mill buildings which are planned to include mixed uses. These buildings serve as a satellite business district for the Town of Clayton. This offset results in a less than desirable condition and a safety concern.

The entry to downtown from US 70 Business was re-configured and a downtown monument sign was installed in 2010.



Main Street / US 70 Connection (2001)

Development opportunities can be found on maps LA-3 and LA-4.



Post Office

### Railroad Corridor

The railroad corridor, which parallels Main Street throughout the project area, carries a 100-foot right of way to each side. Lying within this right of way are First Street from O’Neil to Lombard Streets, Main Street itself at the intersection of Main and Second, and Front Street on the eastern / northern side of the railroad corridor.

The future of this railroad corridor includes a second track to be located within the corridor. The Town of Clayton and the North Carolina Railroad have a landscaping agreement that provides guidelines for maintaining landscaping within the rail corridor. New plantings of trees or shrubs higher than 3’ present sight distance issues for motorists at grade crossings and for train personel and are restricted as outlines in the agreement.

A January 3, 1990 agreement between Southern Railway and the Town of Clayton allows for the maintenance of an unpaved roadway (commonly referred to as “First Street Extension”) extending southeastwardly from North Chruch Street for a distance of approximately 520 feet. The agreement should not be construed as a dedication of this portion of the rail corridor for a public street.

The Master Plan map (LA-2) shows a downtown greenway connection at North Lombard Street and the rail. Any improvement to this at-grade crossing or increase in the use of this crossing by pedestrians will neccesitate an under-grade or over-grade separation.



Railroad Corridor

## Design Guidelines

The guidelines are intended to add consistency and predictability to the permit review process. Proposed developments within the Central Business District shall exhibit design elements that contribute to the unique sense of character of Downtown Clayton.

In response to our observations and community input during the process the philosophy behind our solutions and the recommendations illustrated in the master plan reflect the following goals:

- Identify and portray opportunities for new development
- Improve pedestrian orientation of Main Street
- Identify and define parking areas
- Create a community space
- Establish a sense of entry at key approaches to downtown

## Development Opportunities

Development opportunities include new construction and the adaptive reuse of existing buildings. Opportunities for new buildings are present along Main Street, Second Street and First Street with the removal of incompatible and noncontributing buildings in addition to utilizing currently vacant lots. From Church Street to past Lombard Street is defined as the center of downtown.

While opportunities along Main Street should serve primarily retail, office, and commercial space, opportunities along Second Street could accommodate residential land use. Buildings along Main Street could certainly accommodate residential use on upper floors. Along First Street buildings could exhibit a mix of uses.



First Street / Adaptive Reuse of existing buildings

## Design Guidelines

### Pedestrian Orientation

Future development should provide priority treatment for pedestrians in the design of transportation facilities. The pedestrian environment should be improved by making it easier, safer, more convenient and more comfortable to walk between businesses, on sidewalks, across streets, and through parking lots. Pedestrian facilities such as sidewalks and crosswalks should connect all modes of transportation.

Where possible, sidewalks should be provided along both sides of streets for a variety of activities that accommodate and complement downtown life. A variety of pedestrian areas to accommodate shoppers, residents, employees, and visitors should be provided.

Unfortunately the dimensions of Main Street will not allow the introduction of angled parking spaces on either side and continued traffic flow in both directions. To provide some

parking on Main Street while improving the pedestrian emphasis parallel parking is limited to one side. [parking is currently allowed on both sides of Main Street.]

Pedestrian orientation is improved by the creation of pedestrian corners or refuges at street intersections and mid-block crossings.

By taking control of Main Street from NCDOT the incorporation of special paving can be utilized to draw further attention to the pedestrian. These areas could also be raised to create traffic tables which will calm and slow traffic.



Pedestrian Orientation

## Design Guidelines

### Pedestrian Connections

The development of pedestrian connections between buildings from parking areas behind to Main Street is a key component.

Suggested connections include:

LA-1: Between Ellington & O'Neil St  
From Clayton Center to Main St

LA-2: Between 208 & 218 E Main St  
Between 220 & 222 E Main St  
Between 226 & 232 E Main St  
Between 236 & 312 E Main St

Pedestrian connections should also be strengthened between downtown and neighborhoods to the north of downtown across the railroad tracks and neighborhoods to the south by extending sidewalks.

All pedestrian connections from Main Street to Front Street require approval from NC Railroad Company. Grade-separated crossings are preferred by NCRP while at-grade crossings are discouraged. Either way, it is recommended that formal pedestrian crossings be made between Fayetteville and Lombard Street.



Enlargement of map LA-1

## Design Guidelines

### Parking Areas

The location and design of parking areas or structures should create a safe, convenient, and attractive network for vehicle circulation and parking.

The master plan illustrates the development of parking areas behind the buildings fronting on Main Street. The formalization of these parking areas by paving will result in more spaces that are better organized.

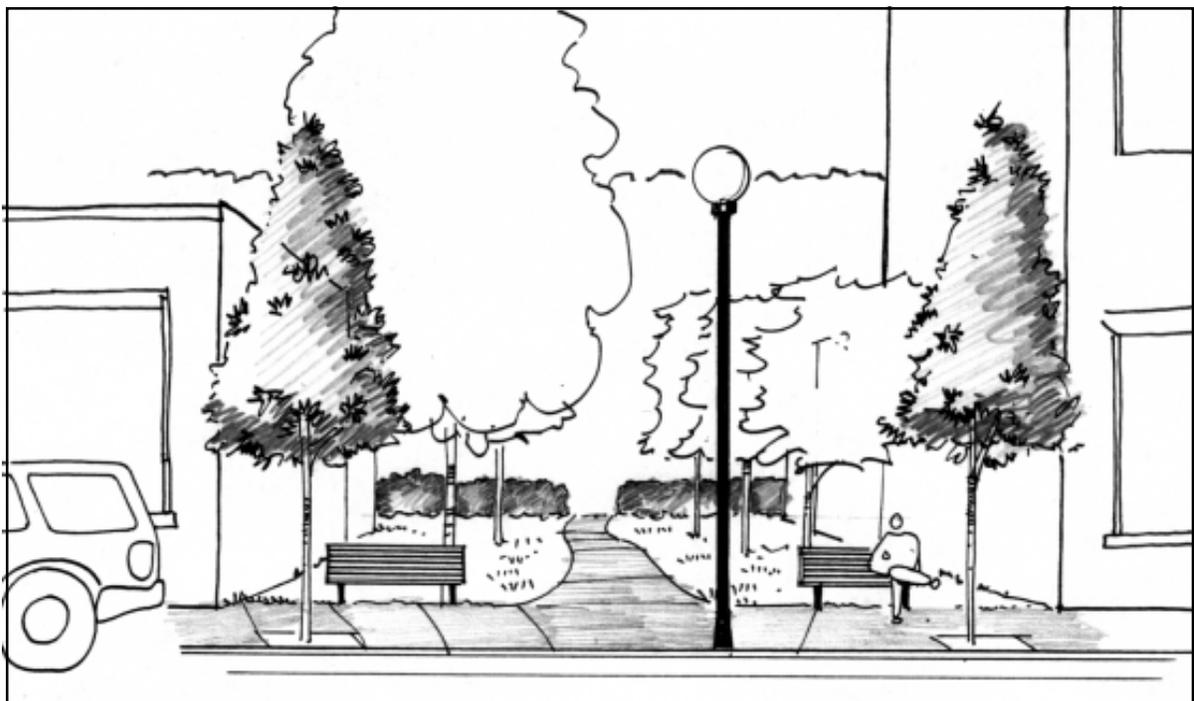
The configuration of parking areas will require a cooperative effort between property owners and / or the Town of Clayton.

With the removal of parking along one side of Main Street, these lots will supply parking for those who are shopping, eating, or conducting business in downtown.

Therefore, pedestrian connections from parking areas to Main Street should be strong, direct, and convenient without diminishing pedestrian movement or visual experience.

Driveway impacts to pedestrian walks should be minimized.

Adequate parking is required along with the the promotion of transit and other forms of transportation.



Parking / Pedestrian Connection to Main Street

## Design Guidelines

### Community Space

Two significant community spaces envisioned by the 2001 plan have been realized.

The most central community space is at the corner of Lombard & Main streets. The second space is the old school playground block is now known as the Town Square, where public events such as the Town Square Concert Series, Christmas Tree Lighting and even the Clayton Farmers Market occur.

Future community spaces should be planned to be pedestrian friendly and flexible in use. They can serve to connect various uses and / or accommodate specific uses.



Clayton Town Square (old school playground)



Community Space / Corner of Lombard and Main

## Design Guidelines

### Adjacent Properties

New development should promote functional and visual compatibility between adjacent properties and avoid visual or aesthetic contrasts with the style of downtown generally and particularly adjacent properties.

New development proposals should coordinate with surrounding site planning and development efforts on adjacent properties.

New development proposals should also consider the following existing design features to create visual continuity between the proposed development and adjacent neighborhoods and the community:

- building setbacks
- placement of structures
- location of pedestrian and vehicular facilities and spacing from adjoining buildings.

Site design features include composition of plant materials and street trees as well as fencing, walls, lighting, pavement, and other site amenities.

Building design features include scale, massing; proportion; size, spacing and the location of windows, doorways and other features; roof silhouette; facade proportions and orientations; location of entries; surface material, finish, color, and texture of surrounding development; and style of architecture.

In some areas, the existing context is not well-defined, or may be undesirable. In such cases, the new development will be recognized as a model with the opportunity to establish a pattern of identity from which future development can take its cues. Relevant zoning requirements, design guidelines and other

downtown plans and policies shall be considered as indicators of the desired direction for the area and the project.

Proposed development should be properly linked to existing and planned pedestrian, vehicular, drainage, and utility systems, and assure efficient continuation of such systems.

Consider the impact of the development upon existing and planned adjacent public and private open spaces, parks and recreation areas.



Rendering of Proposed Law Enforcement Center

## Design Guidelines

### Gateways / Entries

A major entry from US 70 is defined at the intersection of Main Street and a realigned Second Street. The realignment with Central Street would serve to improve the safety of this intersection while creating the space for the development of an entry feature.

In combination with the Compton House, a sequence of walls and plantings create a sense of entry at this intersection. The plantings should extend between the street and the railroad to continue the sequence towards downtown.

The gateway at Main Street and US 70 Business was realigned in 2008 with enhanced planting, and removal of overhead lines. A downtown monument sign was installed in 2010.

Other major gateways into downtown include Robertson, Fayetteville, and Lombard Streets.



Rendering of Proposed Central Street Realignment



US 70 / Downtown (Main Street) Gateway



Robertson Street

## Plan Components

Physical recommendations of the master plan for the downtown study area reflect goals and objectives developed during the planning process. The recommendations are not described on the basis of the sub-areas used in the inventory and analysis description. The improvements and recommendations of this plan are described in terms of plan components.

Each of the design concepts is linked to the others in various ways. The study's recommendations are defined by the following descriptions of individual components of the plan.

### General Improvements

Improvements common throughout the downtown study area include the placement of overhead utilities underground, a comprehensive streetscape and tree planting program, lighting, and new sidewalks.

### New Construction

Development of buildings in downtown area are infill by nature. New construction or development opportunities are identified all along Main Street and parts of Second Street. Along Main Street, street-level uses should be retail / commercial, restaurants and other services. Upper floors of buildings along Main Street can be occupied by office or residential space. Buildings that front along Second Street should be residential in use and character. Multi-family or attached housing is considered appropriate and essential to building density in the downtown area. Streets perpendicular to Main and Second will offer limited opportunity for structures which face the street. This is due to the dimension and depth of the block in addition to the desire for mid-block parking areas.



Overhead utilities

New construction should reflect and respond to the rhythm created by existing structures as well as their detail and scale. While there should be consistency with regard to the distance from the building to the street curb, new buildings could be set back slightly to allow for wider sidewalks and outdoor dining. Buildings in this area could be as high as 4 stories and remain compatible with existing structures. New construction shall conform to the Architectural Guidelines found in Section 2 of this report.

### Demolition

No building should be demolished without first securing a demolition permit from the Town of Clayton. The permit process should include review and sign-off by the Downtown Development Coordinator. Demolition of any contributing building in the National Register District is strongly discouraged.

## Plan Components

### Adaptive Reuse / Historic Preservation

The reuse of existing buildings is the most effective method of historic preservation and is key to preserving Clayton's past and enriching the character and appearance of the downtown area. The use of these structures follows similar guidelines and scenarios to those described for new construction. Residential structures displaced for new construction can be moved to areas with more similar surroundings. Remnant structures or architectural elements should be preserved or recovered and reused as part of public space design.

Historic buildings which are currently home to a viable business should be preserved.

A good example of this is the conversion of the old elementary school and high school into a community arts center as well as town administration offices (The Clayton Center). The Chamber of Commerce building is a good example of adaptive reuse. This approach should be extended to many buildings along Main Street as well as the old Town Hall building.



Horne Building

### Streetscape Guidelines

Streetscape improvements are physical solutions that address pedestrian safety, appearance, parking and traffic movement.

The features that form the basis of the streetscape include pedestrian refuges at downtown intersections, pedestrian crosswalks, and a palette of coordinating materials and elements which create character. The pedestrian refuges create space for parallel parking along Main Street and provide the opportunity for street tree plantings, a place to sit, as well as additional space for the pedestrian to wait before crossing the street. The refuges are directly related to crosswalks. The paving material across Main Street for the crosswalks strikes contrast with the asphalt to bring further attention to the pedestrian. Pedestrian refuges, crosswalks, etc. should incorporate accessible design features.

The elements of the streetscape which contribute to the overall character of the street include but are not limited to the following:

## Plan Components

### Paving Materials:

Paving materials should be authentic and modular to allow for utility repairs with no disruption of the overall appearance. Sidewalks should comply with the streetscape plan

### Lighting Fixtures:

Lighting fixtures should be pedestrian in scale and classical in design safety is improved with street lighting and character is further defined. Lighting fixtures other than traditional cobra heads are encouraged.

### Street Furniture:

Street furniture should be of a durable material and complimentary to the lighting. Street furniture includes benches, trash receptacles, kiosks, bollards, decorative clock or landmark kiosks, etc. All elements to be approved by the Town of Clayton.

### Plantings:

Existing plantings include holly trees. These trees are viewed as an asset, however, their relationship to the sidewalk surface needs to be urbanized. New street plantings should be shade trees.

### Artwork:

Artwork can be integrated into public streets and pedestrian improvements. The location and design of these elements should not impede views or hinder pedestrian traffic.

### Water Features:

Water features or decorative fountains are encouraged. These features should be visible and accessible from a public walk or space. They should be designed to use water efficiently.



Example of Pavers

### Signage:

Informational or directional signage in the downtown area should give clear direction to parking areas, shops and other civic buildings. Signage should be uniform and reflect the character of the street furniture. This includes freestanding signs and signs mounted to buildings.

Other elements, which may be part of the streetscape or incorporated into design of downtown facilities include fences, gates, and screening devices. In those areas of downtown characterized by non-residential buildings the need for fences and gates is less than those areas with a residential feel.

Screening devices to address loading docks, services entries, trash receptacles, including dumpsters, and other undesirable elements and views should be compatible with the materials of the adjacent buildings.

Two keys to implementing the streetscape improvements is the removal of Main Street from the roster of roads controlled by and maintained by NCDOT and the placement of overhead utilities underground.

## Plan Components

### Railroad Corridor

The NC Railroad corridor runs parallel with Main street through most of the study area. It separates the core commercial area of downtown from the largely residential area north of Front Street. This physical separation should be minimized through grade separated pedestrian access improvements. The railroad right-of-way extends 100 feet to each side of the current tracks. Located within this area currently are streets, improved and unimproved, and some buildings. Although the location of the future second track has not been determined, some of this right-of-way could be used for locating parking and improvements including plant installations, subject to NCRR approval. It is felt, while the first 50' from the track should be clear of any obstructions, the outer 50' can be utilized as open space or for parking and landscaping again, subject to NCRR approval.

First Street from O'Neil to Lombard should be improved, subject to NCRR approval. The exception to buildings within 50' could be a transit station at Lombard or south of town closer to US 70 Business, but the Town would need to coordinate with the planning efforts of NC-DOT Rail and NCRR. Remaining right of way to the north and south of this area should be landscaped on both sides of the tracks. There is a desire to locate a commuter rail station as close to the downtown commercial district as possible.



Church Street Railroad Crossing

The Church Street connection across the tracks should be improved for pedestrian traffic, as it is most central to the neighborhood.

When the railroad corridor and Main Street come closer together in the area of Second Street, the area between the street and the tracks should not include parking only landscaping.

## Plan Components

### Parking Areas / Structures

Off-street parking, where provided, should be restricted to the side and rear of any principal building. Parking areas adjacent to street rights-of-way should be separated from the sidewalk / street by a minimum three foot landscaped strip, except for approved driveway curb cuts.

Driveways are limited to one entry lane and one exit lane per 300' of street frontage. Vehicular access to corner lots with less than 300' of street frontage should be located on the lowest classified street and as close as practical to the property line most distant from the intersection, subject to NCDOT standards and specifications.

A shared access easement (vehicular and pedestrian) may be required between adjoining parking lots at the same grade to minimize the number of access points along streets and facilitate traffic flow.

Parking areas should be separated from buildings and public sidewalks with a landscape buffer. Combinations of ornamental fencing and plant material should be used whenever possible to minimize the visual impact of large expanses of parking lot from public view.



Parking Areas

Create employee parking in remote areas to allow for customer parking closer to the building entrances. Employees should be prohibited from parking on the street.

The bulk of a parking structure as seen from the street should be minimized by placing its shortest dimension along the street edge. The parking structure should include active uses such as retail or other appropriate uses at the ground level and/or along the street frontage.

## Plan Components

Converting vacant lots to formalized parking areas is primary to the plan. In order for the parking lots behind the buildings to become utilized, the pedestrian connections from the lots to Main Street must be convenient and feel safe.

Parking structures should be architecturally consistent with exterior architectural elements of the primary structure, including roof lines, facade design, modulation, and finish elements.

Buildings built over parking should not appear to “float” over the parking area, but should be linked with ground level uses or screening. Parking at grade under a building is discouraged unless the parking area is completely enclosed with the building or wholly screened with walls and/or landscaping.

### Parking / Pedestrian Connections

Pedestrian and vehicular circulation should comply with the Downtown Clayton Streetscape Design Plan and the Town’s street guidelines.

New Construction should provide obvious pedestrian access onto the site from the adjacent streets and integrate on-site pedestrian circulation with the design of pedestrian facilities on the adjacent street and development.

In multiple-building developments, pedestrian connections should be provided to all businesses and entries of buildings.

In parking areas, pedestrian walkways connecting the parking area with primary building entrances, pedestrian-oriented spaces, adjacent streets, and adjacent properties should be provided at least every 150 feet. Pedestrian walkways should be delineated by separate paved routes using a variation in paved texture and/or color, and protected

from adjacent vehicle circulation areas with landscaping. Approved methods of delineation include, stone, brick, or granite pavers; exposed aggregate; or stamped and colored concrete. Paint striping on asphalt as a method of delineation is discouraged.

Pedestrian alleys should be creatively designed, clean, and functional. Alleys should provide for pedestrian linkages through mid-blocks and between properties. Amenities such as seating and planters should be provided to encourage pedestrian circulation. Whenever possible, a portion of the shop’s storefront should be open to the alley to create a window shopping experience.

A minimum of one mid-block connection is recommended with a minimum 10-20 foot width. These spaces should be well lit and provide spaces for public artwork. All pedestrian facilities should comply with accessibility standards.

The future of Barbour Street is envisioned to take on a more pedestrian feeling as it connects Main Street with the old Town Hall structure and Clayton Law Enforcement Center.



Town Square pedestrian crossing

## Plan Components

### Community Spaces

The two existing community spaces are quite different in character. The central community space at Lombard Street is urban and an expansion of the streetscape while the Town square on the old playground property is more traditional.

The space on the former site of the Horne mansion features gathering and seating opportunities for small groups of people and spaces, which could organize displays at Harvest Festival or other community functions. It also accommodates parking.

One of the design goals of this space was to use many of its elements as a springboard for a streetscape plan for downtown, as well as for small programming events, everyday public use and for parking.

In the long term it is suggested that a building could be constructed between the community space and the railroad. This future building should serve some community purpose such as an open market or transit stop.



Rendering of New Town Lot



Town Square

The community space at the old playground site is a more traditional public square. It is suggested that if other parking areas can be identified in the future to accommodate the community center (adaptive reuse of the elementary school structure), that the use of the entire block be devoted to a traditional community space.

Both of these spaces serve the community in a variety of ways. They provide spaces for gatherings and special events, a green respite during the workday, a place to interact with friends, a place to display Clayton's heritage or public works of art, and a place that serves to strengthen the identity of the community.

## Plan Components

### Gateway / Entry Areas

The gateway to downtown from the south and US 70 is recognized as a primary entry. Recent improvements include a re-alignment of Main Street with US 70 Business, landscaping and a downtown monument sign that sets the standard design for downtown wayfinding and other gateways.

All along the east side of Main Street, and in the area between the street and the tracks should be planted in concert with street lighting and a stone wall which visually leads to the entrance feature at the intersection of Main Street and Second Street. At this location the stone walls enclose the intersection and define a formal green entry space.

The stone walls should resemble the walls that enclose the cemetery on Front Street and can be seen along NC42. These stone walls in combination with street tree plantings and lighting continue along Main Street towards downtown.



Stone wall and column



Entry / US 70

In addition to this gateway, improvements to suggest entry are appropriate for Robertson, Fayetteville and Lombard Streets. At the intersection of Main Street and Robertson the use of stone in the form of walls or entry columns can signify an entry on a more modest scale.

A feeling of entry as one approaches from the west on Fayetteville Street will be created by the Town Square on the old playground property.

Lombard Street features primarily single family residences after it passes below US 70 until one block before it's intersection with Second Street. An open space at the northwest corner of Lombard and Horne Street provides a place for an entry statement. Lombard is recognized as a primary entry to downtown and improvements at this location are appropriate.

## Implementation

Implementing the recommendations of this plan is contingent upon the policy for review of future construction projects as well as the dedication of the town leadership to initiate town projects.

Any new construction considered for Main street or within the project study area should be evaluated according to the recommendations found of this plan. A formal review of the project to insure its compatibility is necessary.

Public projects initiated by the town should also reflect the recommendations of this plan.

The Clayton Historic District has been listed on the National Register of Historic Places and several incentive programs have been adopted for new construction and facade improvements to assist in the preservation of the existing buildings as well as generate interest in the downtown area as a place to invest. Partnership between the public and private sectors is essential for continued improvements



## Table of Contents / Section II

1.	Purpose of Guidelines	II-2
2.	How to Use the Guidelines	II-3
3.	Elements of a Downtown Clayton Building	II-4
4.	Sequence of Investigation and Work	II-6
5.	Architectural Guideline Topics	II-8
	• Windows	II-9
	• Doors	II-14
	• Facades	II-19
	• Storefronts	II-24
	• Rhythm	II-29
	• Materials	II-32
	• Proportion / Scale	II-37
	• Roofing	II-42
	• Awnings & Canopies	II-43
	• Signage	II-48
	• Color	II-53
	• Exterior Lighting	II-56
	• Modernizing	II-57
	• Infill / New Construction	II-61
	• ADA Accessibility / Upgrades	II-65

## Appendix

A.	Workshop #1	A-2
B.	Workshop #2	A-14
C.	Maintenance Checklist	A-20
D.	Additional Resources/Publications	A-25
E.	Federal Tax Credit Summary	A-26
F.	Secretary of Interior Renovation Standards	A-29
G.	2010 Workshop Materials	A-30

## Purpose and Objectives of Guidelines

The following architectural design guidelines have been developed to assist in the recognition and maintenance of the historical character of Downtown Clayton. The guidelines are intended to promote the preservation of existing significant buildings while encouraging new construction to be compatible and supportive of the existing context. The intended objective is to have functional and visual compatibility throughout the downtown area without creating any negative impacts to adjacent properties.

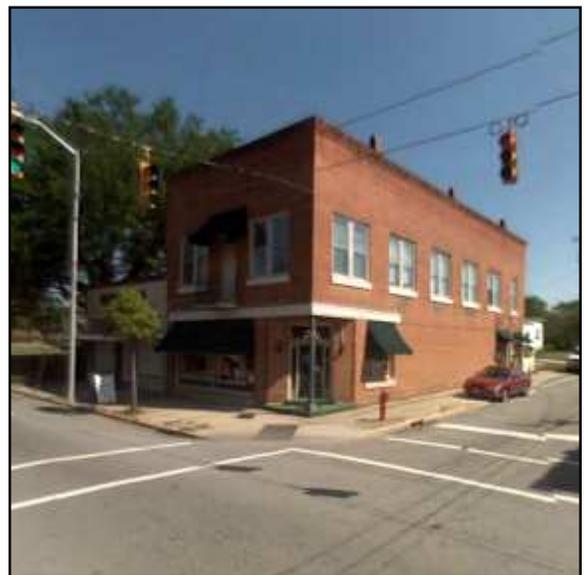
The architectural guidelines are not intended to be all-inclusive, but rather suggest the main topics that are typical and common to the downtown area. Within each topic there exists both narrative descriptions, recommended and not recommended guidelines, and supportive graphics. By utilizing all the information, the reader is encouraged to explore the various topics that pertain to their particular situation. Incorporating the guidelines into individual projects will give each property owner a common ground of understanding.

The guidelines are not intended to produce a Clayton that is homogeneous and void of any variety. Downtown Clayton has always boasted a wide array of businesses and building styles. Such a variety is important for the continued vitality of Downtown Clayton.

Incorporation of these guidelines will be an important step in assuring that the character, history, and uniqueness that makes downtown a special place will continue for years and generations to come.

Through continued efforts of proper maintenance of existing structures, appropriate renovations, and sensitive new construction, Clayton will continue to enjoy a true town center that is vibrant and rich in architectural character.

While these architectural guidelines focus primarily on physical building elements, it is equally critical that any new or rehabilitation project coordinate with the surrounding site planning and design objectives of the overall downtown master plan. Incorporating and respecting certain site design features, placement of structures, coordination of vehicular and pedestrian, and the spaces created between structures are considerations that will affect the context. (Please reference section I for more detailed information.)



## How to Use the Guidelines

The architectural design guidelines are organized into 15 main topics, with each topic being detailed to include introductory text, a recommended, not-recommended summary, and graphics to visually reinforce the topics. In addition to the guideline topics, there is additional information concerning maintenance procedures and building checklists. Background information on historic tax credits and a resource list is also included.

To begin working with the design guidelines, a property or business owner in the downtown area would start with an evaluation of his/her own property. Once identification of key elements and conditions have been noted, then the reader is encouraged to look at the various topics in the guidelines that apply to their property or anticipated work. While these guidelines are based on the Secretary of Interior Standards for Rehabilitation, each building and project is unique and should be undertaken by considering not only the subject property, but the adjacent context as well. Each building and property in the downtown plays an important role in creating, reinforcing, and maintaining the character of the downtown. It is a character that is unique to Clayton.

When undertaking actual rehabilitation work or new construction in a historic setting, it is always advisable to seek the advice of professionals, to employ individuals who have experience with rehabilitation work, and have familiarity with referencing design guidelines. Professional experience, guidance, and assistance covering aspects from design to construction will likely result in a better finished product for the owner and the community.

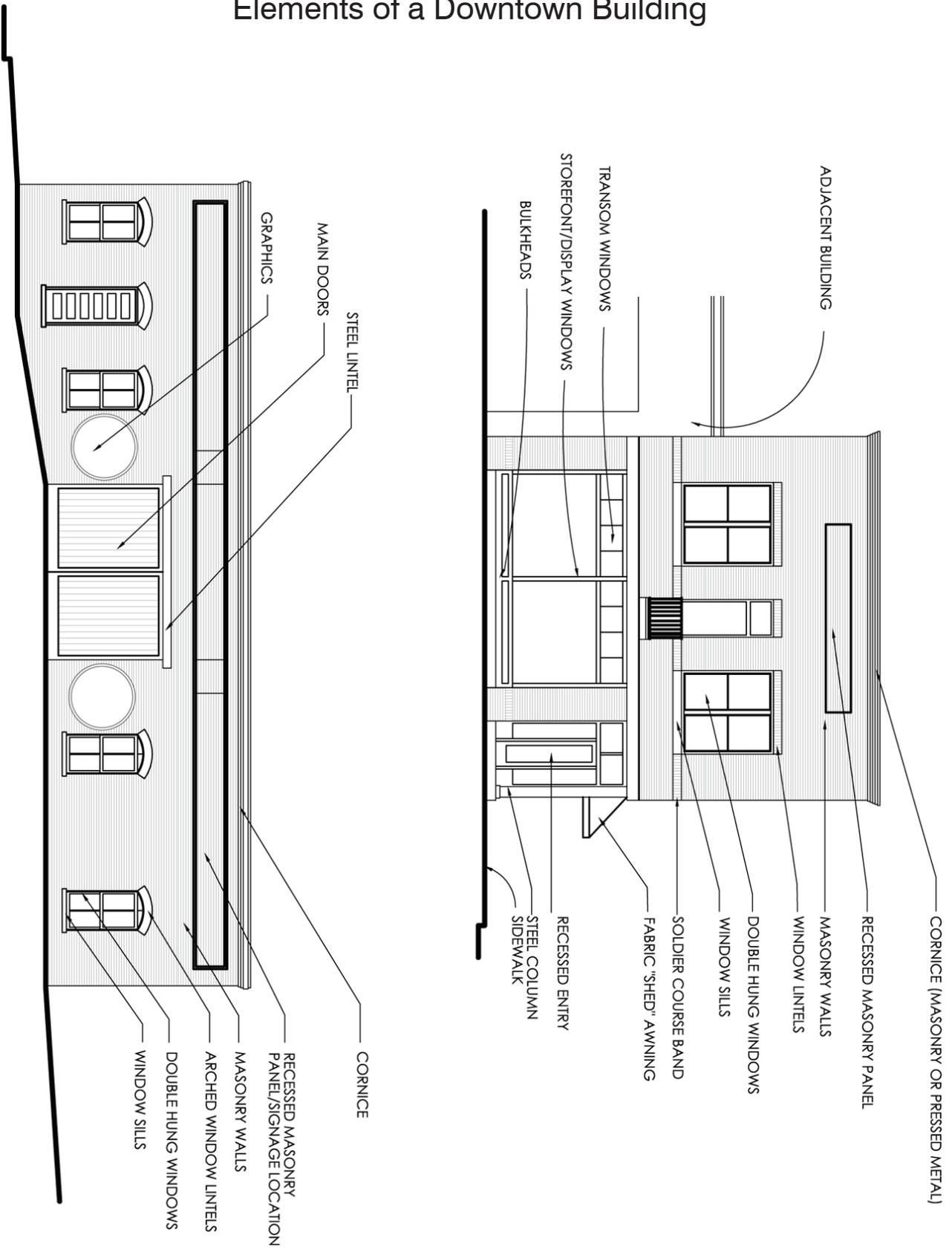


## Elements of a Downtown Building

The following graphic depicts the front elevation of two different buildings in Downtown Clayton. The elevations of the single-story and multi-story building are noted to identify architectural elements that are common to many downtown structures. This visual and written description is important in understanding the language of historical buildings.



# Elements of a Downtown Building



## Suggested Sequence of Investigation and Work

1. Inspect the roof for leaks, also checking gutters, downspouts, copings, and flashing. Water infiltration into a building from the roof is one of the most damaging and harmful conditions that can happen to a structure. What may begin as a minor inconvenience can quickly become a major problem leading to deterioration and structural damage requiring major repairs.
2. Investigate the structural integrity of the structure, paying special attention to identify any damaged areas, including walls, floors, and ceilings/roofs. Repair any damaged structural supports and verify that any rotten or termite-damaged wood has been removed and replaced. Before undertaking any repair work, it would be advisable to document the existing conditions, taking special care not to remove any historical materials without first understanding the impact to the historical character of the structure and any planned historic tax credits.
3. Survey existing windows and doors. Repair and repaint any damaged or exposed wood. Unpainted wood surfaces will deteriorate at a high rate, leading to eventual failure of the material. Replace broken window panes and repair damaged hardware. Check to see if any metal flashing is installed above the window and door heads and that the window sills are properly draining water away from the bottom of the window. Recaulk window jambs if required. Replace existing windows when necessary, following the design guidelines and conditions to maintain integrity of the façade and supportive of any possible historic tax credits.

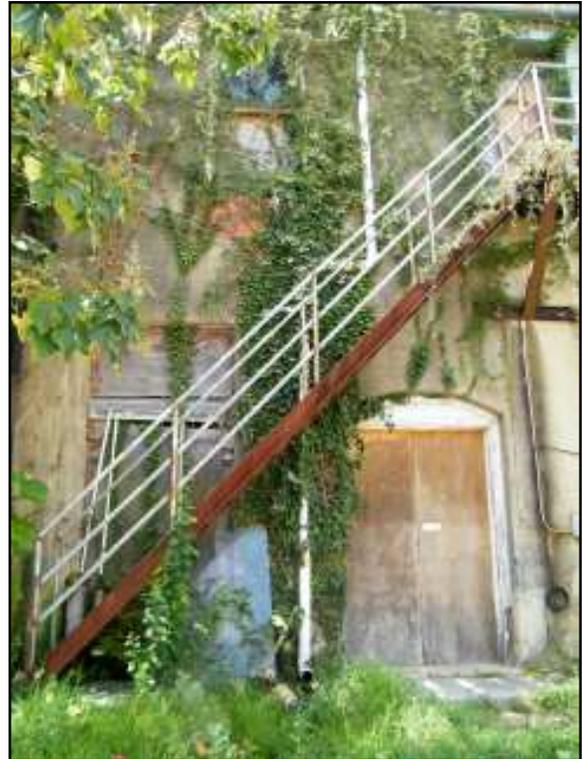
4. Clean existing masonry wall surfaces, following instructions to use the most gentle means of cleaning possible. Strong chemicals, high-pressure washing, or sandblasting are all cleaning methods that could cause irreparable damage to the masonry and mortar joints.



5. Repair any damaged exterior wall surfaces. Repoint masonry following technical guidance from either National Park Service Technical Guidelines or North Carolina Archives and History. Use of a mortar that has a higher strength than the existing mortar could result in damage to the existing masonry. Any missing masonry should be replaced with masonry to match the existing.

6. Survey existing issues that pertain to handicap access and conditions of accessibility to the existing building. In some cases a minor modification may greatly enhance and improve the use of a building by persons with disabilities.
7. Investigate the existing storefront and repair/repaint as necessary. When possible remove any materials or coverings that are not original and historical in nature. In some cases, the damage and change to an existing storefront may be so significant, that a new storefront may need to be constructed.
8. Replace damaged or out-of-character awnings and signage.
9. Remove any abandoned wires, sign brackets, or foreign out-of-character façade materials. Take special care not to damage any historical building materials or facades.
10. Fabricate and install new awnings.
11. Fabricate and install new signage.

Note that activities beyond evaluations and that will involve physical changes to the structure should be reviewed by the Clayton Downtown Development Association as part of an ongoing effort to foster communication and cooperation among business and property owners in the downtown area.



The following fifteen topics represent the architectural design guidelines for Downtown Clayton. Some of the topics involve simple, single building components, while others involve more complex, “bigger picture” items. In any case, some or all of these may apply to a particular building or project. At the same time, however, all of these topics are integral to the overall downtown as a whole. The information presented in each topic is certainly not exhaustive but should give a basic overview and benchmark to guide decisions. As with any opinion, there is always room for interpretation as long as decisions are made considering the interest of all involved.

The guidelines intend to promote and encourage high quality rehabilitation and development in Downtown Clayton. Incorporation and use of these guidelines solidify and support the unique identity of Clayton in a positive manner and will ensure that Downtown remains a viable and active destination for work, play, and living.

## Topics

- Windows
- Doors
- Facades
- Storefronts
- Rhythm
- Materials
- Proportion/Scale
- Roofing
- Awnings/Canopies
- Signage
- Color
- Exterior lighting
- Modernizing/Updates
- Infill/New Construction
- ADA/Accessibility Updates

## Windows

Windows represent one of the most prominent features on any building. The buildings in Downtown Clayton are typical examples of turn-of-the-century and twentieth century designs where windows were not only a design feature, but were necessary for the function of the building. Historically windows have served to provide ventilation and lighting, both at a time prior to the invention of air conditioning and wide-use of electric lighting. Although there were variations in styles, most of the windows typical for downtown structures were large glass areas on the first floor retail, and smaller, individual windows on the floors above. Above first floor windows were usually operable, double hung, and constructed using wood sashes, trim, various mullion patterns, and single-pane glass.

When addressing the issue of window repairs, care should be taken to preserve the original sashes, trim, and moldings. Any repair work should blend with the existing windows and not be apparently different. Replacement and new windows should be consistent with the existing style of the window that is being replaced or in context with adjacent windows. Proportions and sizing should fit into the existing wall openings.



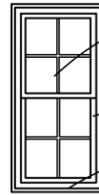
## Windows: Recommended

- Provide on-going maintenance and protection of existing windows by painting, caulking, and weather-stripping
- Repair any damaged window frames and sashes by splicing in material that matches in type and profile
- Replace any existing window with one that exactly matches the former
- Replace an existing window with a similar, accepted style and glazing pattern. If replacing the window, then all windows should be replaced to provide a consistent façade
- Cut in new windows if required to match existing rhythm and setbacks of adjacent windows
- Hold back any interior ceiling height changes to allow for original full-height windows
- Window glazing should use clear glass and not tinted, opaque, colored, or spandrel materials
- Any fixed window system should be compatible with the dimensions, colors, patterns, and proportions of existing or adjacent windows
- Use wood, vinyl-clad, or painted metal for new windows. Bronzed or natural aluminum frames should not be used
- Any window mullion pattern should be applied to both interior and exterior planes
- Any storm windows should be constructed with wood storm sashes. Residential-type aluminum storm windows should be discouraged
- Design any required security devices (metal bars) to compliment the windows
- Windows should be no lower than two feet above the finish grade
- Double hung windows are encouraged to have a height-to-width ratio of 2:1 for upper stories
- Windows are recommended to have horizontal separation of less than fifteen feet from the nearest adjacent window or door in the same façade plane
- Provide window openings with stone, brick, or cast concrete lintels and sills
- Set window frames and glass back from the building façade consistent with other downtown historic construction

# Windows Recommended



1:3 MULLION PATTERN



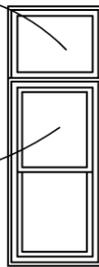
4:4 MULLION PATTERN

APPLIED OR INTEGRAL MULLIONS

PAINTED WOOD OR VINYL CLAD TRIM

WOOD SILL

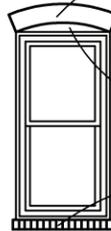
FIXED TRANSOM UNIT



TRANSOM WINDOW

OPERABLE WINDOW

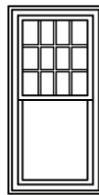
ARCHED MASONRY HEAD



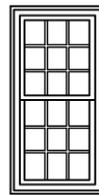
DOUBLE HUNG WINDOW

WOOD INFILL PANEL

BRICK ROWLOCK SILL



12:1 MULLION PATTERN

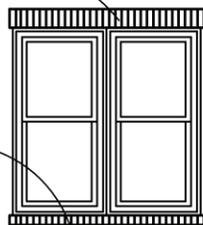


9:9 MULLION PATTERN



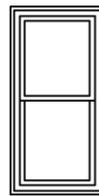
2:2 MULLION PATTERN

BRICK SOLDIER HEADER

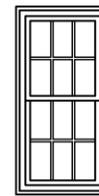


WINDOWS GROUPED

BRICK ROWLOCK SILL



SINGLE 1:1 WINDOW

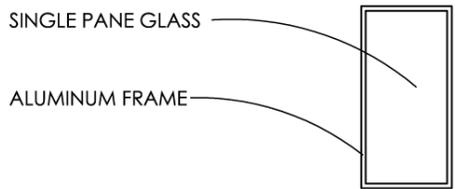


6:6 MULLION PATTERN

**Windows: Not Recommended**

- Removal of windows that are part of the overall historic character of the façade
- Replacing operational windows with non-operational, fixed window units
- Using frame material that is not in keeping with the original historical character, including bronzed or aluminum storefront framing
- Using any vinyl window products
- Adding shutters on commercial buildings
- Covering, boarding-over, or in-filling existing windows and openings
- Changing the window shape
- Utilizing a window or mullion pattern that is not in keeping with the overall character
- Removing original trim, ornament, or other features
- Boarding-over or blocking windows from the inside
- Neglecting normal maintenance, repair, and upkeep of existing windows
- Adding aluminum storm windows
- Using a glass block-type material in existing openings if the building is not of that time period
- Adding aluminum residential-type insect screens to commercial buildings

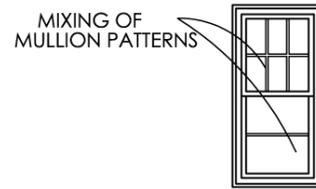
# Windows Not Recommended



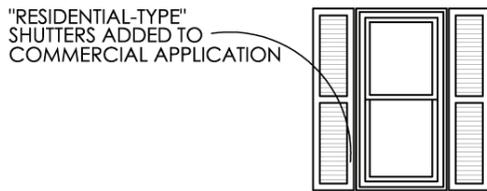
INOPERABLE WINDOW



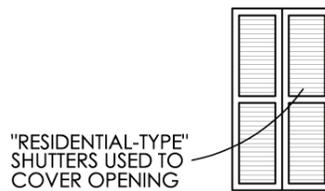
INOPERABLE WINDOW



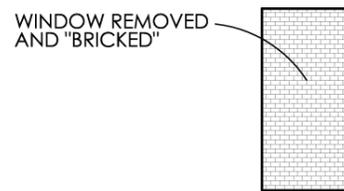
INCONSISTENT MULLION PATTERNS



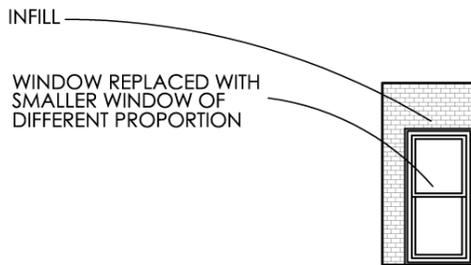
APPLIED DECORATION



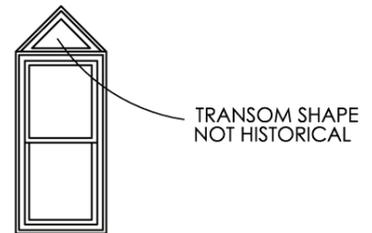
REMOVED WINDOW



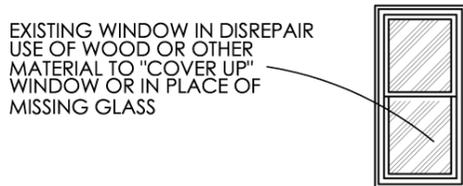
REMOVED WINDOW



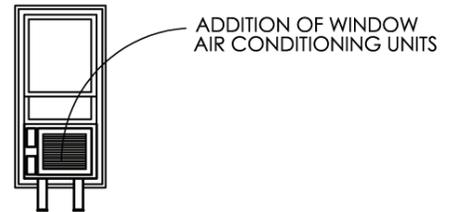
REPLACED WINDOW



INCONSISTENT WINDOW SHAPES



UN-REPAIRED WINDOW



EXTERIOR HVAC ADDITIONS

## Doors

Of the many different elements that compose a building, the door becomes one of the most important. From a pedestrian, building-user point of view, no other element receives such close inspection and use than a door. Doors are objects that a user can inspect at close range and is most familiar with in terms of types, materials, operation, and hardware. In Downtown Clayton, the front door is a gateway to the retailer's shop or an office area. As such, it is important that the door be perceived as an element of the storefront and/or building façade and not as a separate entity.

Historically, doors have been wood and have ranged from the simple, to the more elaborate. Doors have been solid or contained glass, usually including some type of trim and paneled design. More contemporary doors have been simplified and contained a higher percentage of glass. With the introduction of aluminum as a building product in the middle twentieth century, many replacement doors and windows minimized any type of frame and maximized the amount of glass area. Current door manufacturer's have recognized the renovation movement and most offer a complete line of new doors that reflect the historical character of earlier designs.

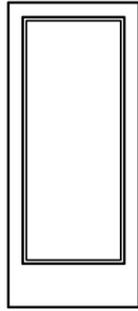
By their placement and usage, doors typically receive considerable wear and tear. The selection of proper hardware, appropriate finishes, and on-going maintenance is critical to insure the longevity of a door.



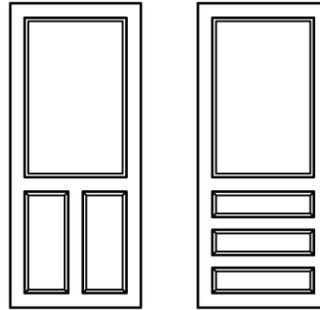
**Doors: Recommended**

- Repair and refinish existing historical doors
- Install new hardware that is accessibility-friendly
- Paint existing doors and perform other types of on-going maintenance to insure operation, protection, and longevity
- Improve energy efficiency of existing doors by caulking and installing weather-stripping
- Provide handicap accessible thresholds
- Construct new replacement doors to match existing historical doors in material, size, configuration, scale, and shape
- Use old photographs or other historical data to guide designs for replacement doors
- Integrate new doors with any storefront replacement design
- Use simple design, narrow frames, and painted finish
- Utilize appropriate construction materials, remembering that wood has been historically the material used in early downtown buildings
- Incorporate decorative finish hardware such as kickplates where previously used
- Use clear glass in door openings
- Provide necessary handicap accessible clearances where possible

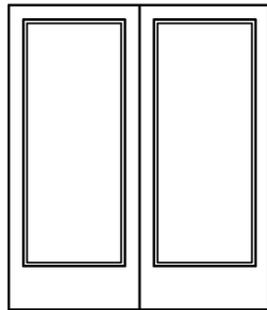
## Doors Recommended



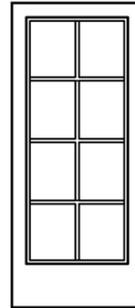
WOOD AND GLASS



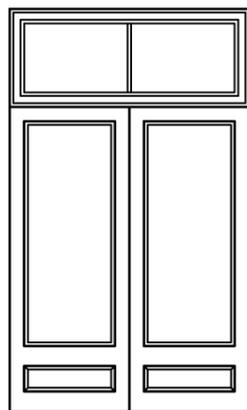
WOOD AND GLASS  
W/ PANELS



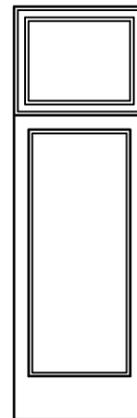
DOUBLE DOORS



DIVIDED GLASS PATTERN



DOUBLE DOORS  
W/ TRANSOM

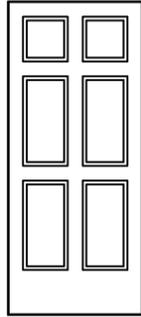


SINGLE DOOR  
W/ TRANSOM

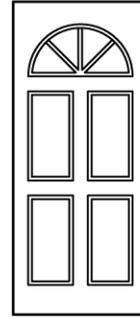
**Doors: Not Recommended**

- Residential-type doors
- Commercial, standard aluminum storefront doors
- Repairing existing doors with materials that are not in keeping with the historical character
- Altering the original style to one that is not compatible with historical character
- Utilizing hardware that is not commercial-rated
- Using shapes or configurations that do not compliment the historical character
- Installing doors that are not predominantly glass

## Doors Not Recommended



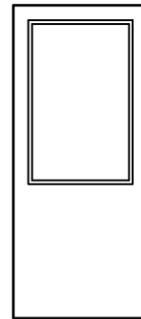
RESIDENTIAL TYPE DOOR



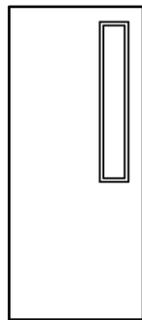
RESIDENTIAL TYPE DOOR



SOLID METAL DOOR



HALF GLASS, NO  
RAISED LOWER PANEL(S)



NARROW SIDE LITE GLASS



ALUMINUM AND GLASS

## Facades

The public facades of the buildings in Downtown Clayton are critical in defining a historical context and character for downtown. It is the consistency, placement, detail, and repetition of the front facades that define the urban edge between the street, sidewalk area, and the buildings. Through a variation of storefronts, windows, materials, colors, and details, the overall image of “downtown” is created and reinforced. Historically, the front facades received the most design attention with the use of better building materials, better windows, and more elaborate detailing. Buildings located on a corner occasionally addressed both the front and side elevations in the same or similar manner. Main building entries occur within the major elevation.

All the elements of the façade are important. Each element is significant and contributes to not only the overall appearance of the single building, but the entire streetscape.

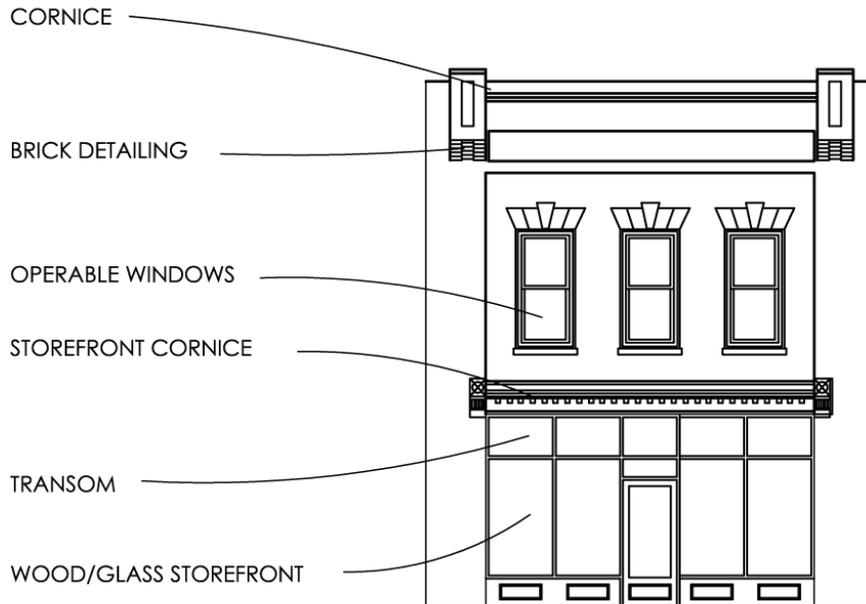
Any work on downtown facades must first focus on the maintenance and repair of existing facades and the appropriate rehabilitation of other facades that have been altered in such a way diminishes their contribution to the surrounding historical integrity. Facades most visibly tell the history of the building and in an effort to improve a façade, attention must be given to avoid adversely altering the building’s history. By the same token, the development of new in-fill facades must also contribute to the historical character of the neighboring facades and structures. Attention must be paid to the materials used, the type and placement of openings, and the incorporation of vertical proportions.



**Facades: Recommended**

- Identify the unique elements of each individual façade
- Provide an on-going maintenance program to insure stability of existing façade materials and elements
- Repair and replace any damaged materials with in-kind and similar materials
- Retain original façade materials
- Utilize old photographs when available to reconstruct a previously-altered façade
- Remove incompatible alterations, materials, and additions
- Understand history of an existing building before beginning any alteration work
- Investigate original paint schemes when repainting an existing façade
- Rehabilitate/renovate an existing façade utilizing the Secretary of Interior Standards
- Employ the services of a design professional when undertaking a façade renovation or new construction
- Clean, re-point, and repair any damage masonry
- Maintain all existing openings and install new openings only in a manner consistent with the existing facades
- Investigate possible tax credit assistance for renovation projects
- Use materials in new construction that are consistent with existing historical context
- Respect adjacent façade scale, proportions, and styles
- Incorporate the prevalent pattern and spacing of façade elements, including windows and doors that are consistent with buildings typically constructed prior to 1950
- Provide a street level façade with window patterns that have a minimum of 20 percent open area and a maximum of 50 percent
- Incorporate proportions that are typically vertical
- Use special detailing and features to highlight a building entry

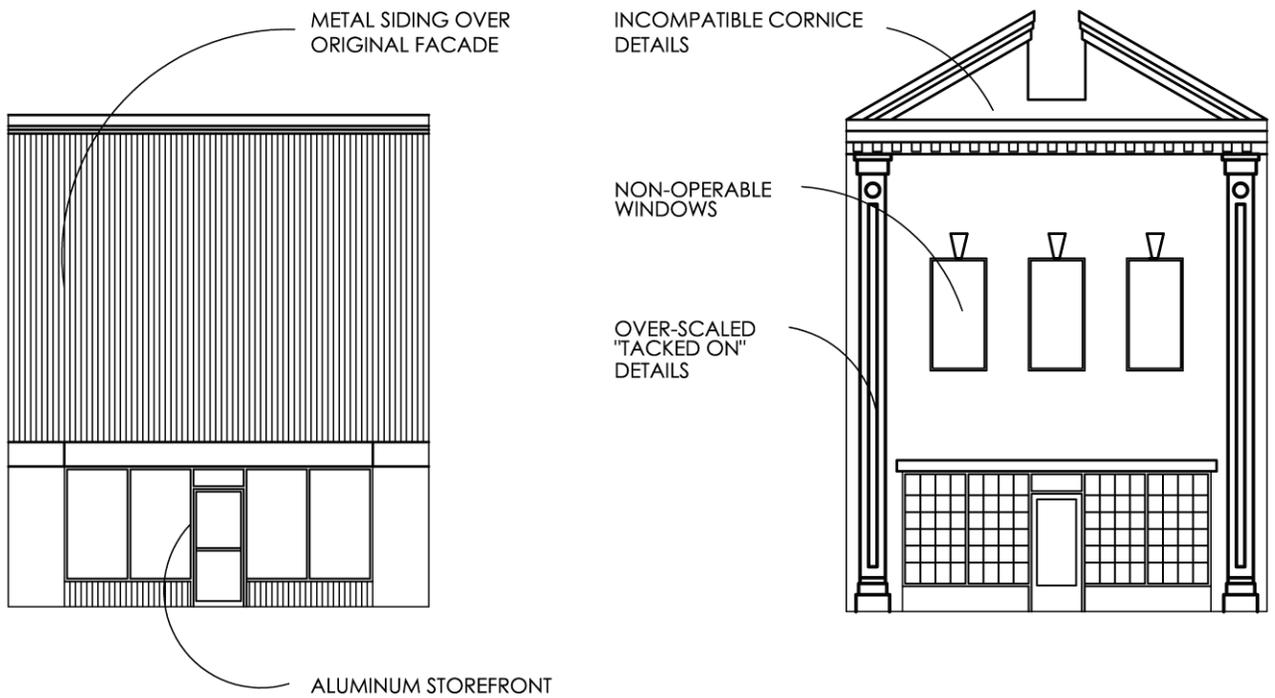
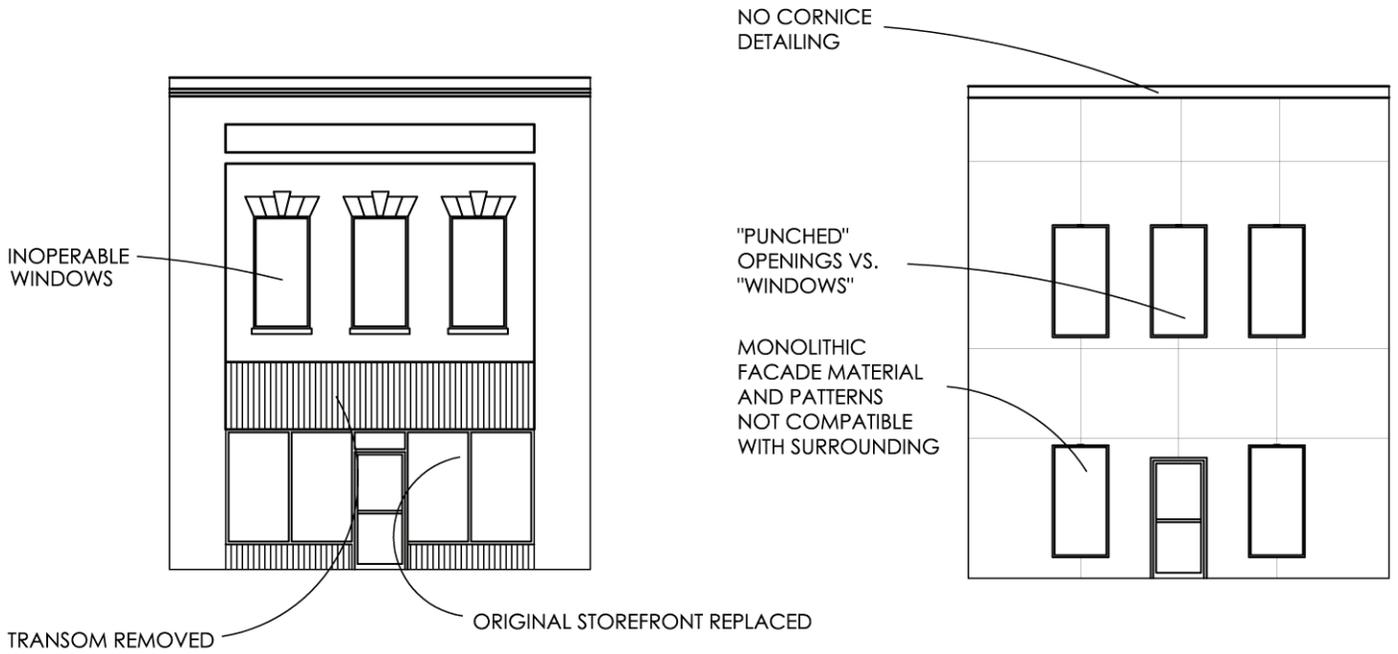
# Facades Recommended



**Facades: Not Recommended**

- Removing any details such as parapets, cornices, or ornamental masonry-work, transoms, lintels, and storefronts
- Not providing for an on-going maintenance program to insure the stability and preservation of existing facades
- Painting previously unpainted masonry without historical evidence supporting past evidence of a masonry façade being painted
- Removing existing windows or door units that are original to the façade
- Adding any new façade material that is not in keeping with the original façade
- Adding new details that are either out of context or incompatible with the original design
- Replacing existing building elements with elements that are not similar
- Altering existing window openings and window patterns
- Adding awnings, canopies, or signage using materials and details that are not historical and compatible
- Window and opening spacing greater than twelve feet separation between elements
- Redesigning an original storefront that does not use similar materials and proportions to the original
- Creating an alteration that would change the apparent time period of the original façade
- Conducting any potentially harmful cleaning process that could lead to damage of existing façade materials
- Undertaking any façade renovations slated for historic tax credits without prior review and approval by the required review agencies

# Facades Not Recommended



## Storefronts

The storefront area, typically facing the main street, is usually an open area of glass with the sole purpose of displaying either the retail wares of the store, or the first floor activities of the building occupant. The storefront can be thought of as a series of display windows. It is very different from the solid walls and punched window openings usually found on the floors above the storefront. Historical construction of the storefront area has allowed between 60 and 80 percent glass with the remaining areas being framed with usually wood or masonry materials.

There are many examples in Downtown Clayton where above can readily be seen. The incorporation of transom windows above the door line to maximize interior lighting is balanced by the use of solid panels and detail below the windows.

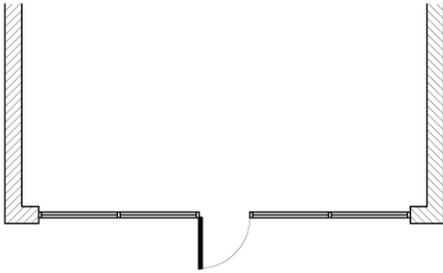
Existing storefronts can be maintained, repaired, or in the case of severe damage, can be replaced. Storefront replacements should either replicate the former design, or be compatible by utilizing typical vertical and horizontal proportions along with existing materials and colors. Storefront replacements or new construction should avoid the use of materials such as bronze or aluminum pre-finished materials and glass should be transparent.



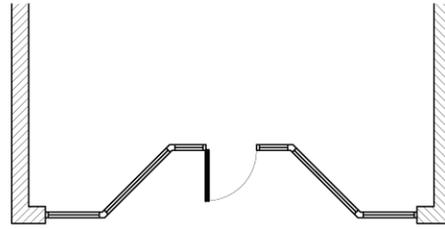
**Storefronts: Recommended**

- Maintain and preserve existing historical storefronts
- Repair existing historical storefronts when damaged
- Integrate overall building color scheme with storefront
- Design new storefronts to be “pedestrian-friendly” and encourage street level activity
- Incorporate recessed doorways at building fronts
- Orient storefronts and entry ways that face the main street frontage and are accessible from a public sidewalk
- Storefronts facing two streets should consider both frontages as front elevations for the building
- Use windows to appropriately display and advertise for the occupant/business
- Protect and secure historical storefronts prior to beginning any rehabilitation work
- Photograph and document any existing conditions and adjacent context
- Remain flush with the floors and façade above with the exception of recessed entries
- Replace a deteriorated storefront with either an “in-kind” replacement or use substitute materials and designs which complement and interpret the original design and neighboring context
- Replace incompatible, out of character materials with contextual materials
- Paint existing aluminum or bronze finishes to blend in with other surrounding wood trim and metals
- If not an accurate recreation of the historical storefront, new designs should be simple and maintain the commercial nature of a downtown structure
- Integrate signage into overall design of the storefront
- Locate main building entries on primary public street and sidewalk.
- Buildings facing Main Street, First Street, Second Street, Lombard Street, Barbour Street, Church Street, Fayetteville Street, O’Neil Street, Ellington Street, and Robertson Street should consider the street frontage as the main, with the storefront exhibiting the qualities consistent with entrances and pedestrian access

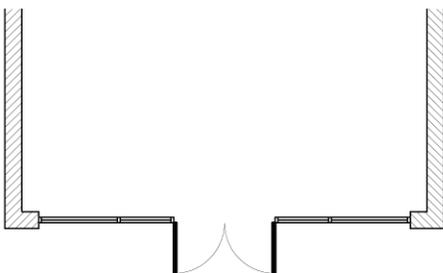
## Storefronts Recommended



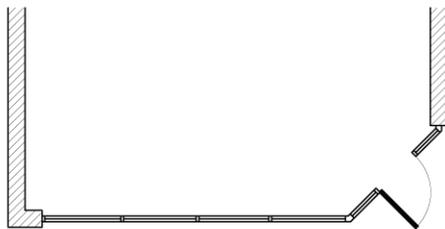
SINGLE DOOR ENTRY



RECESSED ENTRY



DOUBLE DOOR ENTRY

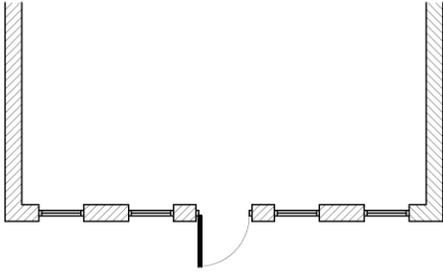


CORNER ENTRY

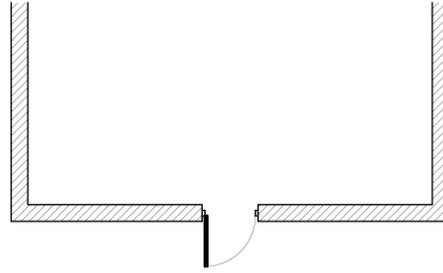
**Storefronts: Not recommended**

- Removing, demolishing, or altering an existing storefront that would decrease the historical character of the building
- Replacing a storefront that could be repaired
- Relocating the original storefront entry point
- Eliminating a special feature such as a recessed or angled entry
- Removing historical details from the original design
- Adding items such as lighting, materials, and details if they cannot be identified as historical and in context with the time period and/or surrounding structures
- Covering over existing windows and transom features
- Creating storefronts that do not integrate doorways and entrances that complement the overall building front and discourage pedestrian activity
- Replacing damaged storefront materials with other materials that may be foreign to the district and surrounding buildings
- Using materials other than wood or painted metal for storefront, door, and window framework
- Introducing colors and signage that are incompatible with the building and the neighboring storefronts
- Using glass that is not transparent
- Utilizing glass block in locations that are not consistent with the original building
- Including solid metal panels in the design scheme

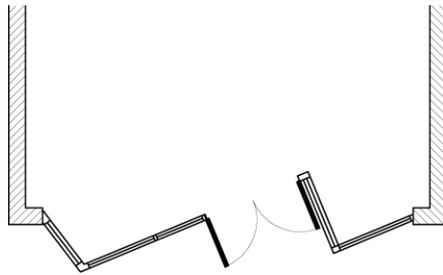
## Storefronts Not Recommended



SMALL STREET LEVEL GLASS



NO STREET LEVEL GLASS



IRREGULAR, NON-HISTORIC SHAPES

## Rhythm

The rhythm of a building or streetscape refers to the pattern that is made by the combination of windows, doors, proportions, and other details that make up the overall façade. Historically the rhythm is the variety that is created between window sizes and spaces, the amount of wall area that is visible, and the placement of design details.

In Downtown Clayton rhythms are evident in not only single buildings, but within whole groups of buildings. Such rhythms are apparent in even seemingly different structures. It is important to recognize that these patterns exist and that any alteration to an existing building or any new in-fill construction should maintain and reinforce that rhythm. At the same time, it must be contextual with the other surrounding buildings.



**Rhythm: Recommended**

- Identify the rhythm or rhythms of the original building so that any rehabilitation work can be compatible and sympathetic
- Use rhythm as a tool to reduce the scale of the building or development to a more human, pedestrian-friendly scale
- Maintain existing window and door openings, including storefronts that contribute to the rhythm of the building and reinforce the streetscape
- Reopen closed window openings that contribute to the building rhythm
- Utilize materials that will reinforce the rhythm of the façade
- Recognize that rhythm can occur in a single building as well as an entire blockface
- Incorporate a rhythm pattern that reflects the traditionally horizontal pattern found in the downtown area
- Maintain or create a clear visual distinction between the street level and upper floors to emphasize the horizontal rhythm
- Incorporate significant architectural elements to help define and support the overall building rhythm
- Utilize / incorporate landscape features to reinforce the rhythm of the building facade or public space

**Rhythm: Not recommended**

- Altering existing window openings and window patterns such that the original building rhythm is altered or nonexistent
- Removing original elements and details that contribute to the overall building rhythm
- Designing new in-fill construction that is void of patterns and rhythms
- Designing new in-fill construction whose rhythm is incompatible with the surrounding streetscape
- Rhythms and patterns that break the traditional horizontal consistency of the overall downtown buildings
- Inconsistent elements that create confusion and busy rhythms

## Materials

Like many other North Carolina towns, the palette of building materials in downtown Clayton has been historically simple and somewhat limited. The availability of timber and the ease and simplicity of working with wood made it the material of choice for many of the original structures in Clayton. Used mostly in residential, there were several early downtown business structures that were constructed with lap wood siding and pitched shingle roofing.

Masonry, particularly brick, however quickly became the predominant material of choice in commercial construction. By the turn of the twentieth century brick was abundant, economical, and there was an ample supply of skilled masons. Coupled with the fact that brick was able to be load-bearing and was considered durable and fire-resistant, very few commercial buildings were built with anything else. With the exception of the Bank of Clayton, stone was not seen in the downtown area as a building material.

Wood continued to be used for windows, doors, storefronts, and decorative trim along with flat plate glass. It does not appear that pressed metal trim and cornices were included on a many of the downtown structures. This is counter to many other areas in the state where this material was regularly used for decoration and coping.

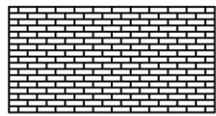
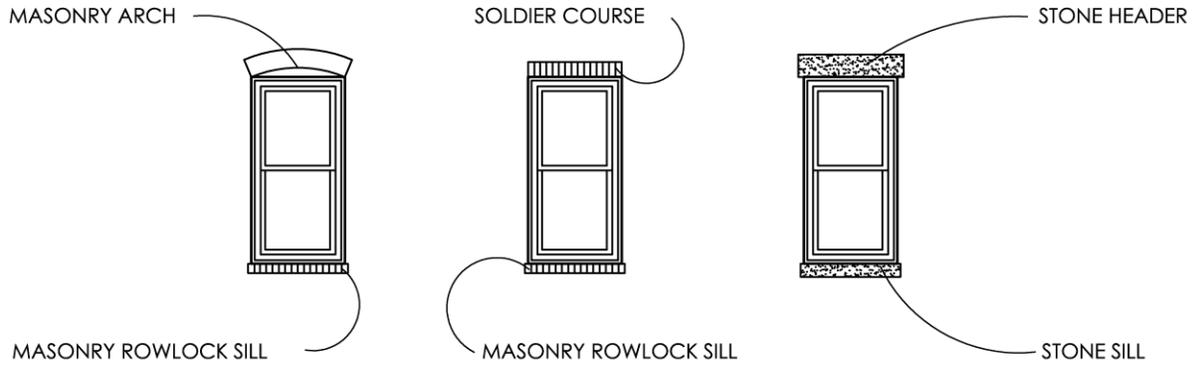
Following World War II, many other building materials became readily available and “fashionable.” In many downtown areas there was a rush to re-clad building facades with either non-historic materials, or to alter the buildings in such a way as to diminish the original historical character. Clayton is fortunate that the majority of downtown structures have not been drastically altered using non-compatible materials.



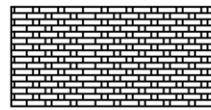
**Materials: Recommended**

- Properly maintain, repair, and repoint existing masonry
- Clean existing masonry using non-destructive methods
- Utilize original materials such as masonry, wood, and pressed metal
- Include high quality and durable new exterior construction materials that reflect the traditional character of downtown architecture including masonry, red brick, and cast stone
- Incorporate contextual brick bond patterns and detailing consistent with historical interpretations and in keeping with adjacent buildings
- Use mortar colors that are neutral, blend with, or are compatible with the brick
- Use consistent, concave mortar joints that match existing joints in texture and strength
- Paint exposed wood
- Utilize special trim shapes in wood to repair existing wood details or in new construction to complement existing
- Include energy-efficient glass in new glazing locations
- Install clear glass in window locations
- Install copper flashing or provide colored flashing to blend with façade materials
- Use materials that are locally available
- Utilize materials that are considered “green” and environmentally-friendly
- Minimize waste and limit disposal of materials

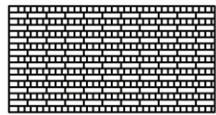
# Materials Recommended



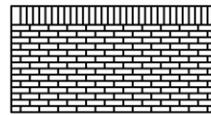
RUNNING BOND



FLEMISH BOND

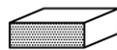


ENGLISH BOND

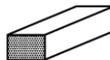


SOLDIER COURSE ACCENT

## BRICK PATTERNS



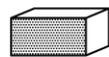
STRETCHER



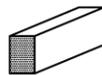
HEADER



SOLDIER



SHINER



ROWLOCK



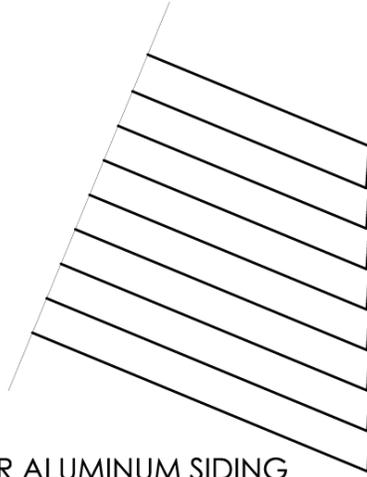
SAILOR

## BRICK ORIENTATIONS

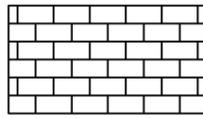
**Materials: Not Recommended**

- Using materials that are not compatible or consistent with the adjacent, existing historical building materials of downtown
- Incorporating materials in an unconventional assembly
- Removing original materials and replacing them with dissimilar materials
- Improperly cleaning or repairing existing materials
- Painting unpainted brick without historical precedence
- Replacing original materials with a synthetic or imitation product, such as vinyl
- Retooling or repointing existing mortar joints with a mortar that does not match the adjacent joints in color, size, texture, and type
- Using raw, unfinished materials such as metal, aluminum, plywood, wood, siding, and concrete within a historical façade or new construction
- Using materials with colors that are not in context with the surrounding materials and finishes
- Using textures that are not in context with surrounding materials
- Using materials that are monolithic and void of a “human” scale
- Using materials that are scarce and not “environmentally friendly”
- Selecting materials that will require excessive maintenance
- Using mirrored or tinted glass
- Using stone/rock, either real or cultured, without historical precedence
- Using metal panels, corrugated metal, fiberglass, metal screening, plastic, or other non-traditional materials as either new construction or to cover over an existing facade

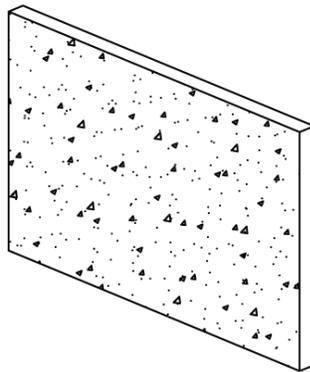
## Materials Not Recommended



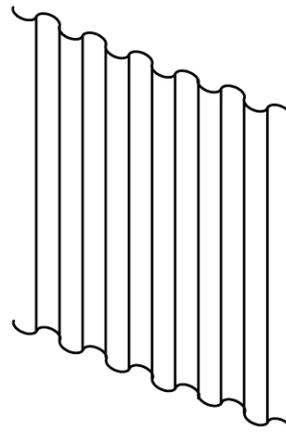
VINYL OR ALUMINUM SIDING



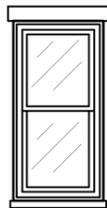
CONCRETE BLOCK



CONCRETE PANELS



METAL PANEL SIDING



REFLECTIVE GLASS

## Proportion / Scale

Proportion and scale are similar to rhythm in the overall composition of each building. It is the proportion and scale of the building that give a sense of size and is usually a comparison to the height of a human figure. The majority of downtown Clayton buildings seem to maintain a well-balanced sense of scale. Detailing, ornamentation, texture differences, interchanging of materials, and use of color are all things that can be incorporated in the overall design of a building to further enhance proportions and scale.

A pleasing proportion and scale will usually aid in making a building “pedestrian-friendly” and relate well to the public. Buildings that are conversely more horizontal than vertical in proportion, are much taller than those surrounding structures, and are void of any detail and rhythm will not be compatible with the historical context of the existing downtown. Larger scale projects are typically better suited for the immediate downtown, while smaller scaled designs are more compatible with the residential character of the structures adjacent to the commercial district.



**Proportion/Scale: Recommended**

- Recognize the existing proportions and scale in the downtown area
- Maintain the height of new additions and construction to stay within a 10 percent range of the mean building height found in the existing block face
- Relate corner sites and buildings on the ends of a block with the height of the adjacent corners and locations
- Width of new additions and construction are encouraged to maintain the streetscape by extending from lot line to lot line with complete building footprint or incorporate architectural walls or other design features to complete the extension
- New construction that spans multiple existing lots should respect the overall width and incorporate a façade design rhythm that supports the progression of adjacent structures
- Renovate or rehabilitate to restore the original scale and proportions found in other downtown buildings
- Use scale and detailing to reinforce the three sections of a façade; base, middle, and top
- Use detail and ornamental design accents to reduce the scale of the building surface
- Use appropriately sized signage and awnings that are compatible with the scale and overall streetscape
- Integrate consistency in reference to and use of scale and proportions
- Use proportions to emphasize main features of a building, such as the entrance
- Selection and configuration of portions to promote a pedestrian-friendly scale
- Break scale of larger building into “smaller” units
- Limit scale and heights of new downtown structures to four stories without additional step backs or off-sets for floors above the fourth level

# Proportion / Scale Recommended



**Proportion/Scale: Not Recommended**

- Large expanses of building materials that is not treated with any detailing, window patterns, or other devices to reduce the scale
- Proportions that overpower and dominate neighboring buildings
- Proportions and scale that are not compatible with the adjacent buildings
- Proportions that are more horizontal as opposed to vertical
- Proportions and scales that detract from the pedestrian scale of the downtown streetscape
- Mixing elements that cause a disjointed and uncomfortable scale
- Sizing of elements that are over or under-sized compared to other adjacent elements
- Proportions and scale that are not pedestrian-friendly
- Large building masses that diminish the individuality of the streetscape

# Proportion / Scale Not Recommended



## Roofing

Although a few early downtown Clayton structures utilized a sloped roof, the typical roof construction for a commercial structure has been a flat design. While considered a “flat” roof, such a roof usually has enough of slope to allow water to drain to one end. The water is then collected at that point into either a gutter with downspouts or into boxed scuppers that become a downspout. Since building fronts are usually considered as the public entry and the rear as service, most roofs have sloped from front to back with the water discharge on the back.

A sloped roof is still common and expected on a single family residential structure. Numerous examples exist in the downtown Clayton area of both the simple and more complex roof forms.

Roofing materials on flat-roofed commercial structures can range from an asphalt-based coating to more contemporary single ply membrane system. Residential roof systems typically utilize fiberglass shingles with a few examples of a standing seam-type metal roofing.

Roofing on commercial downtown buildings is encouraged to maintain the low-profile typical of early to mid twentieth century structures. Typically low profile roof designs are enclosed on three sides with a flat parapet that includes a cornice treatment to cap and articulate the walls. The low side is either left exposed for exterior roof drainage or also includes a parapet if incorporating internal roof drains.



## Awnings & Canopies

Awnings and canopies serve several functions and purposes. While providing the obvious weather protection to the pedestrian, awnings also provide a shield from direct sun on a storefront display window, as well as providing the opportunity for signage and identity. Awnings and canopies are also elements of the storefront and façade and contribute to the scale and detail of the building itself. Several early Main Street structures had actual canopies that extended from the building front out to the street line, providing cover for the sidewalk and a hitching spot for a patron's horse. By the 1920's, canvas awnings, many with retractable capabilities, became a fixture on North Carolina main streets, including Clayton. Most awnings were shed-type and later included materials such as standing seam to wood shake shingles.

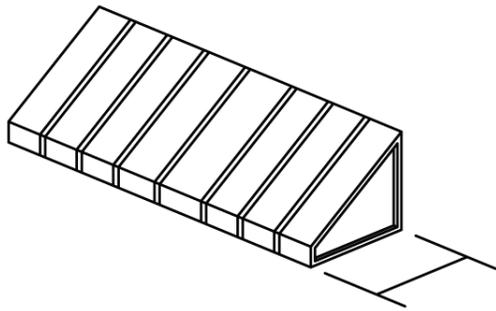
Current building codes are very specific about the types of materials that are permissible. Special consideration should be given to the appropriate size, shape, material, color, and placement of awnings and canopies. Given the prominence of and repetition of awnings that can occur along a streetscape, careful consideration should be given to the replacement of existing awnings or the installation of new ones.



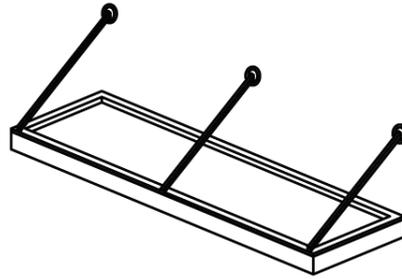
**Awnings/Canopies:  
Recommended**

- Repair any existing historical awnings
- Remove existing fixed awnings if repair or replacement is needed with a fabric awning
- Use new fabric awnings with simple shed profile
- Awning projections that do not project more than three feet from the edge of the sidewalk
- Use awnings that do not appear to be permanent fixtures
- Use awnings that will allow filtered light to pass through to the storefront area
- Use awnings that are open on the underside and do not block any storefront transoms or other historical design features of the building
- Use awnings that incorporate signage including custom-stitched, vinyl, or silk-screened
- Use awnings that are independent from adjacent structures are not continuous
- Use awnings that use colors that are complementary to the building color scheme and to other adjacent properties
- Provide for on-going maintenance and cleaning of awnings
- Review of awning/canopy proposals by DDA

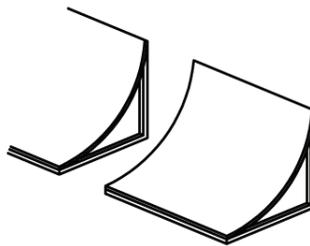
## Awnings Recommended



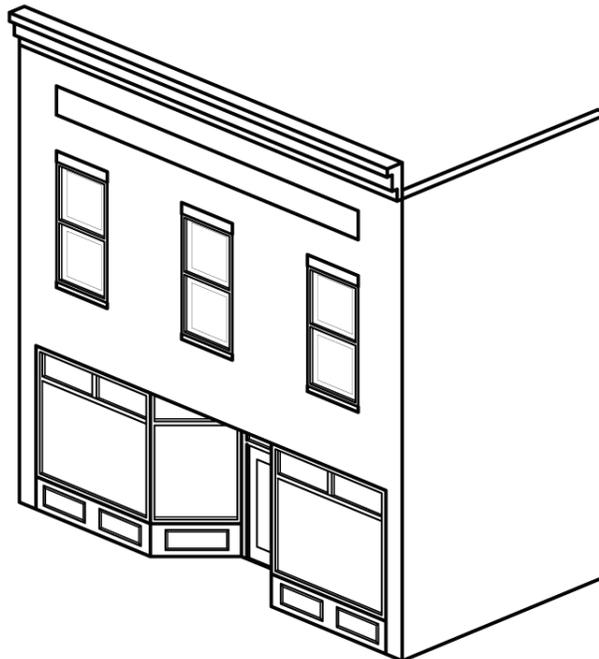
FABRIC "SHED" AWNING



SUSPENDED



FABRIC "SLING" AWNING

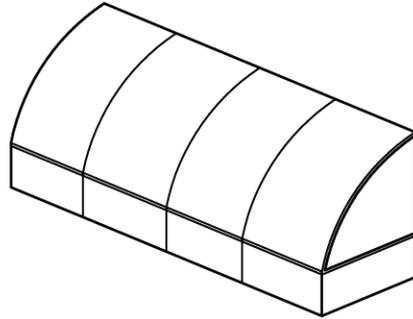


RECESSED ENTRY (NO AWNING NEEDED)

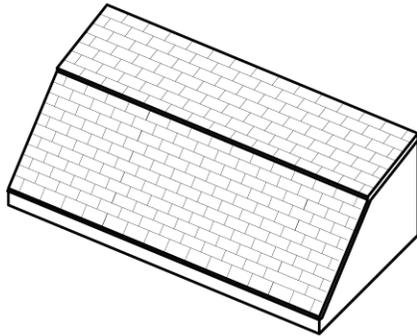
## **Awnings/Canopies: Not Recommended**

- Solid, metal standing seam awnings
- Wood shake or fiberglass shingle awnings
- Awnings that are lower than 8 feet from the sidewalk
- Awnings that project closer than three feet from the street curb
- Awnings that have a “ceiling” on the underside
- Awnings with shapes that are not historical, such as a gambrel or half-barrel vault
- Installations that would cover or damage existing building features
- Backlight awnings
- Continuous awnings that span entire lengths of storefront or bridge multiple storefronts together
- Awnings that do not follow the shape of the top of the opening
- Neglecting a regularly scheduled cleaning and maintenance program for awnings

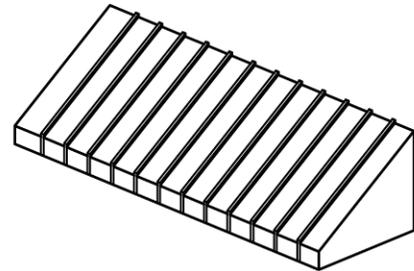
## Awnings Not Recommended



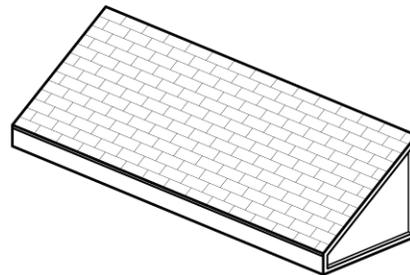
BARREL VAULT SHAPE W/ VINYL COVERING



GAMBREL-TYPE AWNING



SOLID METAL AWNING

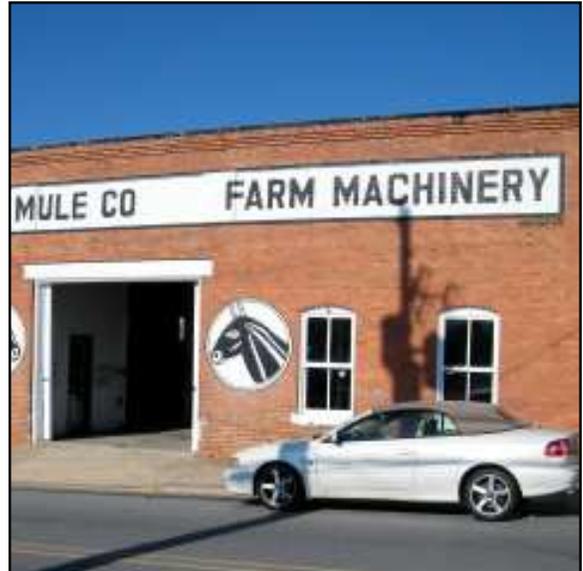


FIBERGLASS SHINGLE "SHED" AWNING

## Signage

There is no doubt that the subject of building signage is a critical and sensitive issue. The need for each individual business to have adequate advertisement must be considered while at the same time preserving the integrity of each building and the overall streetscape. There is a wide variety of signage in Downtown Clayton but is mostly contained on either awnings or the storefront. Some painted sidewall signage exists on a few buildings, several of which are consistent with the time period. Historically, signage appeared on storefront windows, awnings, and within sign bands that were integral to the architecture of the building.

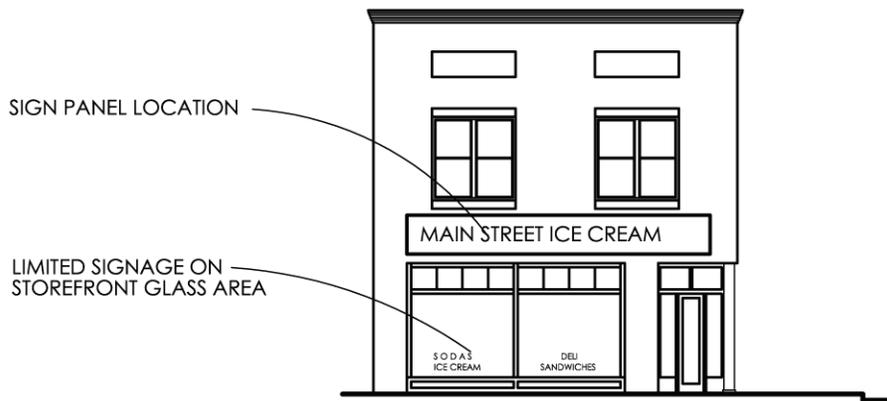
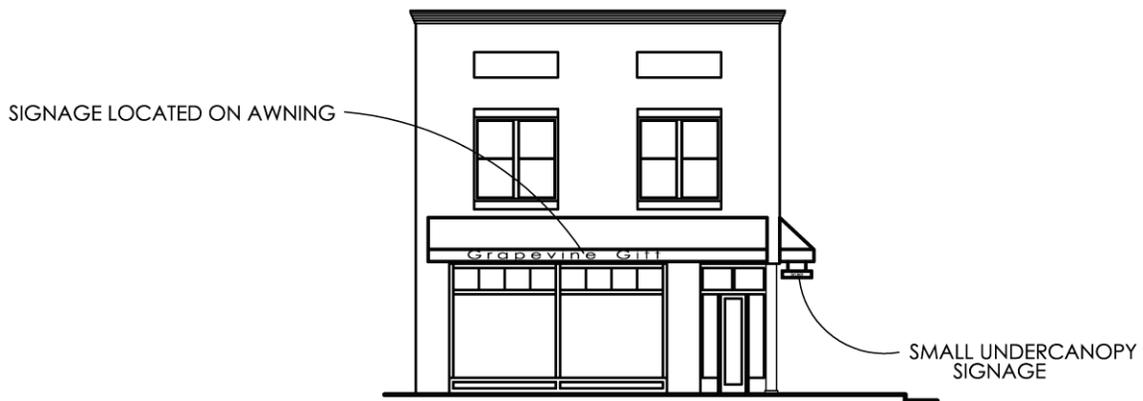
Most municipalities dictate the amount of signage allowed on a business as a function of the amount of direct street frontage for the property. In many cases such as a contemporary strip shopping center development this is controlled and uniform. Within the downtown area there is the desire to have variety, as has been the case in the past. These guidelines do not offer a set standard formula, but do suggest that common sense and respect for the adjacent buildings guide the actual amount of signage that is appropriate. Signage types, styles, and locations that are consistent with the historical context of Downtown Clayton should be the rule.



**Signage: Recommended**

- Preservation of signage that is deemed historic and integral to the use and context of the structure
- Remove and replace overscale, out of character signage
- Integrate signage into the overall design and character of the building
- Use signage on fabric awnings
- Use signage that is simple and does not compete with the building for attention
- Use suspended, non-obtrusive signage located under awnings
- Paint or directly apply signage onto the storefront glass, being sensitive to not occupy large amounts of glass or to conceal the window
- Use colors that are compatible and harmonious with the surrounding buildings
- Investigate history of individual building signage
- Use projecting signs that do not overpower or obscure the adjacent buildings or signage
- Incorporate signage type styles that are compatible with the historical context of downtown
- Allow approximately 50 square feet for single street frontage and 80 square feet for corner locations
- Use signage shapes that are compatible and complementary to the historical style and context of surrounding buildings
- Incorporate traditional techniques such as “gold leaf” edges for sign areas
- Utilize individual letters or “painted” as a block
- Lighting of signage that is compatible with historical context, such as goose-neck exterior lighting
- Review of signage proposals by DDA

# Signage Recommended



**Signage: Not Recommended**

- Neon signage, unless deemed part of a historical signage unit
- Backlit signage or moving lights
- Backlit awnings with signage
- Billboard-type signage
- Over-scale lettering
- Signage that covers existing architectural details or features
- Signage that requires the painting of large areas of existing facades
- Locations of signage in areas that are not historically correct
- Banners
- Large under-canopy/awning signage
- Monolithic “box” signs
- Use of flags, streamers, or other out-of-character elements
- Signage that extends beyond the storefront below or connects multiple storefronts

# Signage Not Recommended



## Color

The incorporation of color is usually seen as an improvement to the uniqueness, architectural character, and adds life to the downtown streetscape. Color can also be a very sensitive and sometimes controversial topic. Historically paint has been used to protect wood and other materials from the negative effects of moisture and deterioration. Some of the early commercial structures constructed wholly of wood likely utilized paint mostly for protection, keeping the color palette simple. Influences from the Victorian period would suggest that the use of color was more elaborate on some of the larger Clayton residences.

By the early twentieth century, brick became the common building material in the downtown commercial area. The inherent “natural” color of brick negated the need for painting a majority of the building body. The use of paint and added color, therefore, became more limited to the wood windows, doors, and storefronts. Color could also be found on awnings and within the business signage.

Early and available paint colors were more limited than today’s almost endless color selections. The paints often were lead-based and tended to be somewhat of a neutral palette. Some colors and color combinations are often associated with a certain time frame and historical era. The reproduction of these schemes can often be successful in the accurate restoration or maintenance of a building façade.

Many of today’s color possibilities, material choices, and styles could not have been imagined when the majority of Downtown Clayton was constructed. It is therefore critical that the use, location, and selection of color be a very deliberate and conscious decision. Historical precedence, surrounding context, and sometimes restraint should all be criteria to consider.



**Color: Recommended**

- Color schemes that express the integral color of the natural building materials such as brick and cast stone
- Color schemes that respect and coordinate with the colors or adjacent structures
- Use of color schemes that are historically appropriate for the age and architectural style of the building
- Use of color to accentuate a particular detail or architectural feature
- Use of color to accent the main pedestrian entry
- Building material colors that blend with other materials found in the downtown.

**Color: Not Recommended**

- Color schemes that do not reflect the historical context of adjacent downtown buildings
- Use of color to highlight insignificant building features
- Use of color only for advertising exposure
- Color schemes that are not coordinated as a unified palette
- Use of “neon” type colors
- Faux paint finishes that imitate natural materials such as marble, stone, wood, or masonry

## Exterior Lighting

Like the topic of color, exterior lighting on downtown Clayton buildings is an important consideration. Historically the buildings of early twentieth century in downtown Clayton were not adorned with exterior lighting. Interior lighting provided for the display of goods and conduction of business and with less emphasis on the more contemporary “extended” hours of business, the need for lit signage and building fronts was not important. The addition of lighting to an existing building or inclusion on new construction should be a complement to the building and the surrounding streetscape as well as being integrated with the overall building design.

Although downtown Clayton does not appear to have a large inventory of examples of “period” lighting, there are specific light styles, types, and general locations for lighting that could be a positive and contributing to the overall downtown character.

Exterior lighting is encouraged to be designed and screened to limit the bleed of lighting to adjacent buildings and sites. The reduction of horizontal glare is important to maintain a balance in general lighting and to improve night sky visibility in Clayton.

While lighting is encouraged for the use and safety of pedestrians and automobiles at building entries, walkways, parking, circulation, and public areas, lighting is not encouraged for advertising purposes.



## Modernizing

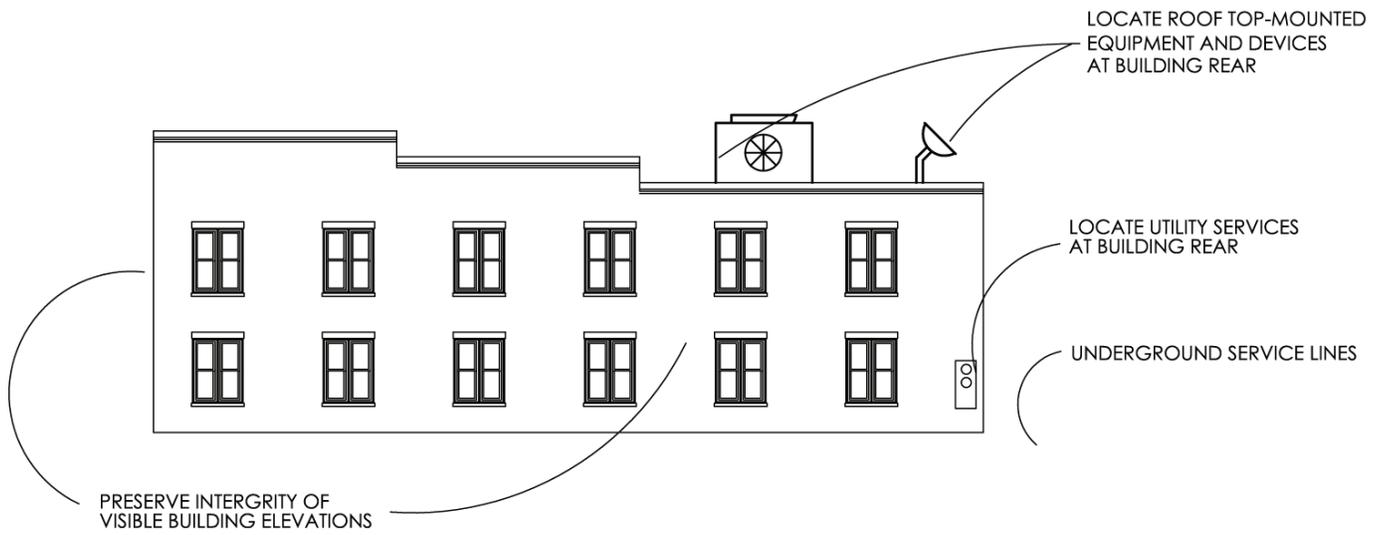
The need for additional air conditioning, electrical service upgrades, and satellite service access are just a few of the “modern” updates that may be necessary in the downtown Clayton area. Careful consideration should always be given to the placement of utilities and equipment in order to minimize impact to the character of the building and to protect the safety and welfare of the general public.



**Modernizing/Updates: Recommended**

- Place utility/service entrances at the rear of the building
- Install roof top HVAC units so they are not visible from the street level
- Install equipment such as vent stacks, antennas, and satellite dishes on the roof so that they are not visible from the street level
- Screen exterior mechanical equipment when in line of sight of the public
- Install utility lines underground when possible
- Install new utilities to avoid damaging or altering historical integrity of building
- Investigate methods for installing new updates on interior spaces to preserve historical finishes and details

# Updates Recommended



**Modernizing/Updating:  
Not Recommended**

- Installing window air conditioning units that would cause damage and alterations to an existing window
- Placing a window air conditioning unit on the front, streetside façade
- Allowing roof-mounted equipment to be visible from the street level
- Locating exhaust discharges that would adversely effect the general public
- Locating rooftop equipment that would obscure the view from an adjacent building window
- Locating any new wire service that would appear on the front façade
- Locating of exterior equipment that is not screened from the public view
- Damaging any building façade material to install new services
- Removing significant interior finishes to install new services
- Installing lowered ceilings under existing decorative ceilings
- Installing inappropriate exterior devices that detract from the building façade

## Infill / New Construction

The development of Downtown Clayton centered around the purchase and subsequent development of individual lots. Whether for commercial or residential purposes, lot acquisitions occurred commonly on a one-by-one basis. With no large-scale planned developments more common in contemporary times, the potential existed for some lots to become developed, leaving other ones open.

Historically, to maximize square footage, most commercial buildings were built to align with the public sidewalk property line at the front. The building sides were built to the edge of the side property, usually resulting in neighboring buildings to either share a common party wall or individual walls that were in full contact with each other. This created a very strong, consistent streetscape line with the only variable being the depth and coverage of the rear yard. With few exceptions, buildings in the downtown Clayton area exemplify this pattern. These consistent lines are critical to the historical urban context and should be respected on any new infill construction that may occur on any currently or future vacant parcels.

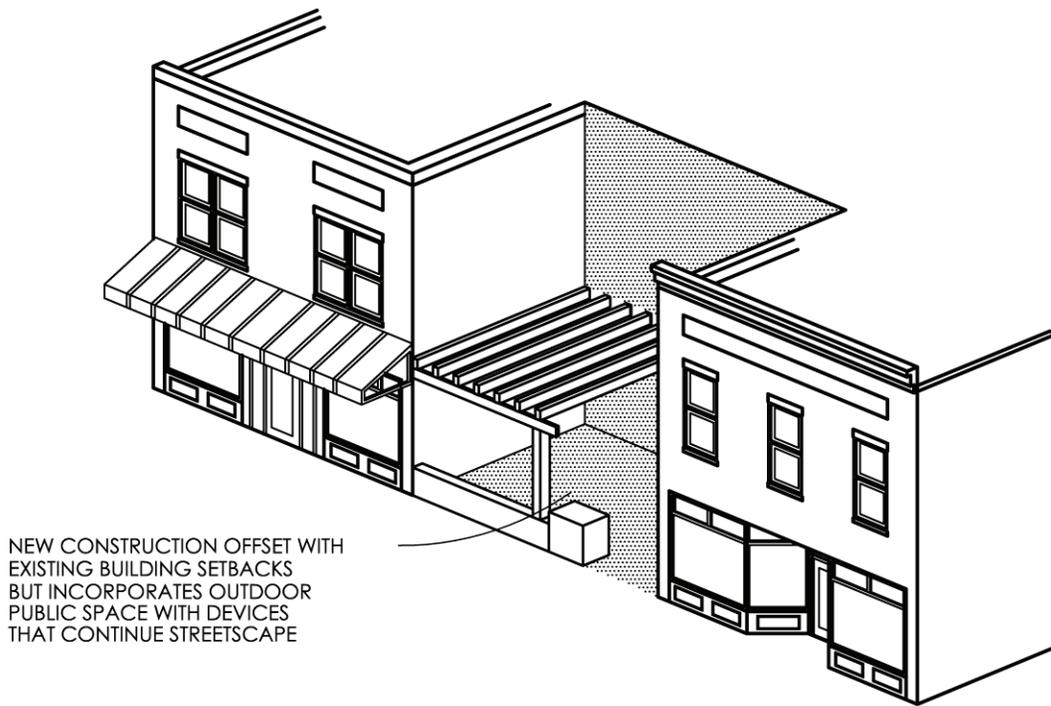
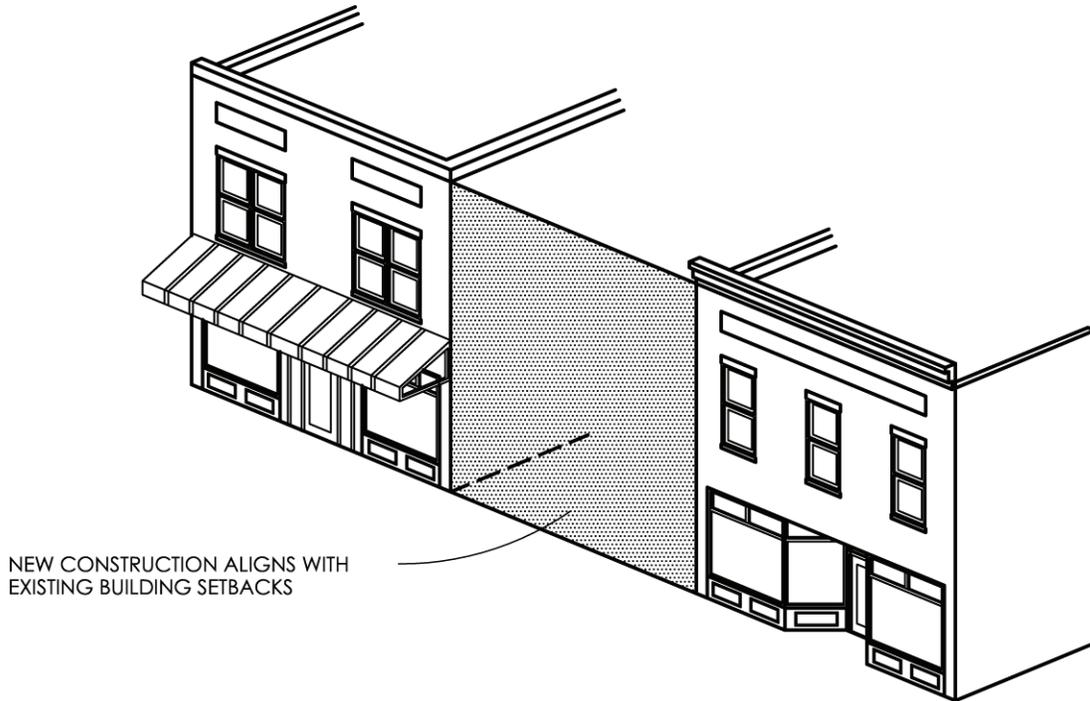
Although residential lots in downtown do not maximize coverage of a lot as a commercial building, there is none the less a typical setback building line, similarity of size and scale, and compatibility of context that would need to be considered and respected in any new residential development.



**Infill/New Construction:  
Recommended**

- Enforce alignments of new construction with the line of existing streetscape facades
- Construct new infill that occupies the full width of the lot in the downtown to create a continuous streetscape
- Follow pattern of existing yard setbacks for locating new construction in existing residential areas
- Construct new infill that responds to existing property lines, especially with any angled corner lot
- Construct new facades to be consistent with the planes of adjacent building facades
- Limit building heights in downtown area to four stories; additional stories possible with upper story setbacks
- Encourage possible mid-block pedestrian connections to parking
- New construction on corner lots should address the corner and utilize the unique location for entry feature
- Integrate variety of materials, window openings, projections, and details to create interest and compatibility with adjacent structures
- Engage pedestrians at the street level and encourage interaction
- Create civic/public spaces where possible
- Incorporate exterior materials that are durable and compatible with the existing historical architecture of downtown
- Encourage new construction and infill to reflect the traditional downtown character where the street is a collection of mostly simple building forms from the early-mid twentieth century
- New construction should become a good neighbor to adjacent buildings and create an interesting and comfortable human environment
- Encourage mixed use and upper floor residential uses.

## Infill / New Construction Recommended



**Infill/New Construction:  
Not Recommended**

- New construction that does not align with adjacent buildings
- Removing existing buildings that will create gaps or voids in existing streetscape
- Constructing new infill construction that create gaps or voids in existing streetscape
- Building exterior walls that do not align with general plane of adjacent buildings
- Creating infill shapes that are not consistent with surrounding shapes and forms
- Allowing overhangs or projecting portions of new construction that exceed any current conditions
- New construction that does not encourage pedestrian interaction
- New construction that is void of sufficient window openings and entrances
- Large, unarticulated building planes
- Incorporating building materials, methods, and styles that are not compatible with surrounding historical context
- Creating forms that do not reflect the simple, three-dimensional character of the historical downtown
- Creating new forms that are not integrated and lack visual unity
- New construction that does not orient to and promote street level activity
- New construction that impedes visual or functional access to an adjacent existing structure

## ADA Accessibility / Upgrades

With the enactment of the Americans with Disabilities Act years ago, the entire country began to address accessibility issues for users of buildings and sites. North Carolina has historically been ahead of the curve and even prior to adoption of ADA has had codes in place to ensure that facilities are accessible and usable by persons with disabilities. Today these codes have all evolved and the North Carolina State Building Code has further merged the state code with the requirements of ADA, ANSI, and the International Building Code.

Given that most of the original downtown buildings were constructed prior to the development of any such accessibility requirements, renovations to existing buildings may present some challenges for building owners and designers to meet new regulations. In some cases, new construction projects may have constraints of the particular site and existing adjoining conditions that will require creative solutions to solve.

Meeting the needs of the general public and following the building/accessibility code should always be in the forefront of any project. At the same time maintaining the historical fabric and integrity of specific buildings and adjoining sites is very important. With the complexities of the various codes and the uniqueness of each case it is advisable that professional consultation be included in any alteration or upgrade to existing facilities. Additionally, property owners are encouraged to evaluate current conditions and make improvements to accessibility when possible.



**ADA/Accessibility: Recommended**

- Review existing conditions that pertain to entries and exits for users of the building and site
- Investigate possibility of providing dedicated handicap parking nearby
- Eliminate abrupt changes in a walking surface of more than 1/2"
- Review North Carolina State Building Code Chapter 11, Making Buildings and Facilities Accessible To and Usable By Persons With Disabilities
- Change door hardware to handicap accessible, lever-type
- Provide adequate doorway widths and clearances
- Avoid obstructions to the path of travel
- Verify that mounting heights of any exterior items such as signs, boxes, lighting, etc... are within limits specified by accessibility code
- Consult with design professionals and building code officials when planning modifications, renovations, or new construction in the downtown area
- Integrate accessibility improvements with the existing building to complement the historical character of the original structure

**ADA/Accessibility:  
Not Recommended**

- Avoiding making improvements when possible to the accessibility of a building, business, or site
- Beginning improvements without consultations with design professionals and/or the building code inspectors
- Renovation or installations that further compromise the accessibility of the property
- Accessibility alterations that are not sensitive to the existing historical character of the building
- Improvements that remove historical building materials and components
- Improvements that segregate the users of the building

## Table of Contents / Appendix

A.	Workshop #1	
	Summary	A-2
	Adgenda	A-6
	Meeting Minutes	A-7
	Current Factors Worksheet	A-8
	Workshop Discussion	A-10
	Workshop Sign-Up Sheet	A-13
B.	Workshop #2	
	Summary	A-14
	Adgenda	A-15
	Sign-Up Sheet	A-16
	Planning Board Update	A-17
	Downtown Development Association Meeting	A-18
C.	Maintenance Checklist	A-20
D.	Additional Resources	A-25
E.	North Carolina Division of Archives & History State Historic Preservation Office	A-26
F.	The Secretary of the Interior's Standards for Rehabilitation	A-29
G.	2010 Workshop materials	
	Vision Statement	A-30
	Invitation List	A-31
	Workshop #1 Adgenda	A-32
	Workshop # 2 Adgenda	A-33
	Current Factors Worksheets	A-34
	Workshop Notes	A-39

## Workshop #1

The process conducted, to develop the recommendations of this plan, include the involvement of the community through two workshops. Workshop #1 took place on Saturday, January 6, 2001, during the day and the second workshop took place on Tuesday February 6, 2001, in the evening.

During the first workshop an inventory of land uses and observations of downtown Clayton were presented for consideration by those in attendance. The participants of the workshop were then divided into four smaller groups to identify and discuss issues facing downtown Clayton. The entire group then reassembled for further discussion.

Issues or factors affecting downtown were rated by the four smaller groups as to their positive, neutral, or negative influence on downtown. The factors evaluated and suggestions made during the discussion were as follows:

### Historic Buildings

Historic buildings were viewed as a positive influence on downtown. It was suggested dur-



Chamber of Commerce



Participants in Workshop #1

ing the discussion that while these structures should be preserved, there was room for aesthetic improvement and that the opportunity for adaptive reuse was an exciting prospect.

### Appearance of Downtown

The current appearance of downtown was rated as a neutral influence. While downtown Clayton was not objectionable, it was felt that there is a great deal of room for improvement. More pride needs to be taken by business and property owners and more trees should be added to parking areas. It was also suggested that clean up efforts by the town could be improved

### Pedestrian Access

Currently many of the attendees feel the pedestrian access is a neutral attribute of downtown. Many participants felt that crossing the street, particularly Main Street, was very dangerous. It was noted further that improvements are needed to address accessibility. Also the width of the streets, where parking along the streets is allowed, seem too narrow, especially when the businesses are receiving deliveries.

## Workshop #1

### New Construction

New construction overall was seen as a positive influence on downtown. One of the main ideas expressed by the participants was that new construction be designed to fit within the existing fabric of the town. It was also stated that some of the existing buildings should be rehabilitated to reflect the fabric as well.

### Public Open Space

Public open space was viewed by the participants as a neutral influence on the town. While the participants agreed that there needed to be more open space they also expressed concern about too much open space. Business owners felt open space could make businesses too far apart and create a lack of continuity along Main Street.

### Parking

The topic of parking was divided into two separate categories: availability and location. Parking availability was seen as a negative, while parking location was neutral. Many business owners stated parking along Main Street is necessary, but that shop owners should not use these spaces because they are intended



Vacant Lot

for visitors. Many participants felt more off street parking was needed; however they felt using the vacant lots in an unorganized fashion, as the lots are currently being used, was unacceptable. Business owners explained that there was a desire to create new organized, paved parking areas with adequate signage to direct visitors to those areas.

### Vacant Lots

The topic of vacant lots was seen by the participants as a negative influence on Clayton. The participants felt the vacant lots should be used for something other than current parking. Some suggestions were parks, paved parking lots, and new construction.



Participants in Workshop #1

## Workshop #1

### Population Growth

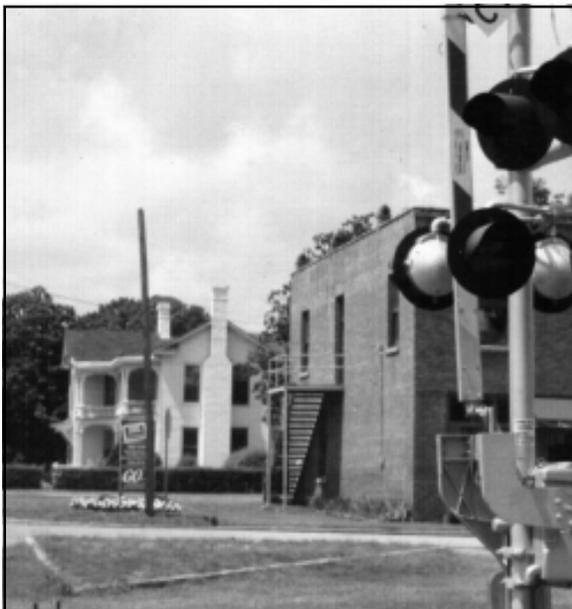
Population growth was seen as a positive influence on downtown and Clayton as a whole. Participants agreed that it was positive as long as it is planned for and helps prevent sprawl.

### Train Tracks

The topic of the existing train tracks fell between the neutral and negative aspects of the downtown area. One of the major concerns expressed by the residents was the safety at the crossings. Another factor brought up by the participants was the fact that the area around and near the tracks often gets weedy and full of litter. Many would like to see some sort of landscaping along the railroad right of way.

### Identity

The identity of Clayton was seen as neutral. Many residents stated that they felt like Clayton did not really have an identity because it is not particularly known for a specific item like antiques, art or a farmers market. In fact, many people who drive the Highway 70 corridor through Clayton do not know that there is a



Fayetteville Street



Workshop #1 Presentation

downtown area. The business owners identified a definite need for more marketing to promote the downtown area and businesses.

### Highway 70

The Highway 70 corridor was seen as a neutral influence on downtown Clayton. Many people commented on the fact that a lot of people use Highway 70, which makes it have certain positive attributes. On the other hand, it was pointed out that the corridor draws business away from downtown and in order to capitalize on the use of the corridor, more signs are needed to draw people downtown.

### Mix of Land Uses / Services

While there are many services offered in the downtown area of Clayton, the participants rated the mix of services as a negative. Suggestions included a desire for more after 5pm activities, retail and restaurants to create more variety. Even though most participants wanted more variety, they emphasized that they felt family owned businesses were very important.

## Workshop #1

### Signage

The topic of signage was viewed as negative. The main problem discussed was the lack of signage. Business owners and residents alike wanted more signs to get people to the downtown area from Highway 70, as well as more signs directing people in downtown. A suggestion was made for the potential installation of a billboard promoting “historic downtown”.

### Safety

While the residents of Clayton feel very positive about the safety in their town, there were a few concerns brought up. Many people agreed that crossing the street was very dangerous and suggested more crosswalks be added increasing visibility for pedestrian crossings.

### Civic Buildings

Civic buildings were seen as a neutral influence on downtown Clayton. It was viewed as positive that the civic buildings were located in the downtown fabric. However, it was pointed out that the current amount of civic space is not adequate enough to meet the Town’s needs.

### NCDOT Control of Main St.

NCDOT control of Main Street was viewed on a whole as negative. It was stated that even though the street is supposedly managed by NCDOT, the town still did most of the repair work and general upkeep. Many residents would like to have Main Street removed from DOT control. One question raised was, “Will DOT reimburse the town for what they will save by not having to maintain the street?”



Main Street at Harvest Festival

### Town Entries

The lack of a defined entry was seen as a negative influence on the town. Participants stated there was a definite need for an entry. Many people suggested NC 42 and Hwy70 as good locations for the entries.

### Traffic Flow

The traffic flow in downtown Clayton is viewed as a neutral to negative influence. Many suggestions were made to help out the traffic flow problem. One was to pave more of First St., extending it from Church Street to Lombard Street. Another idea was to extend Lombard to First Street creating more access to First Street.

January 6, 2001

## **Town of Clayton Downtown Improvements Project**

### **Community Workshop #1 / Workshop Agenda**

09:30 – 10:00	Background and Introduction  Quotes Purpose of workshop Project scope / boundaries / schedule History Strategic plan Work completed to date
10:00 - 10:15	Goals and Mission Statement
10:15 – 10:45	Group Discussion Activity
10:45 – 11:00	Break
11:00 – 11:30	Group Reports
11:30 – 12:00	Additional Items
12:00 – 12:15	Further Discussion
12:15 – 12:30	Next Step in the Process Future Meetings Closing Remarks

Clayton Downtown Improvement Project  
Workshop #1  
Minutes

Public Meeting at Clayton Town Hall  
1.6.01 @ 9:30 AM

Held By: Brian Starkey, Carl Winstead  
Shannon Coleman, & Kimberly Siran

- Brian opened the meeting by introducing the design team.
- He explained the Town's mission on building and revitalizing the downtown area.
- Brian spoke about the local man, Mr. Talton, who in 1909, 1936, and 1961 wrote a small book describing Clayton and life in the town. He read a few quotes from these books.
- Brian spoke of the Clayton Planning Department's Strategic plan for guiding growth in the town.
- Brian discussed the inventory process and the work OBS Landscape Architects and Winstead Wilkinson Architects have completed to date.
- After the scope of the project and the purpose of the workshop were discussed, the group of 33 participants was broken down into 4 groups to participate in a group activity.
- Each group was given three separate worksheets and was asked to rate each of the topics found on the worksheet, as they currently exist in Clayton, as a positive, neutral, or negative aspect of Clayton. (This took about an hour to complete.)
- After the group activity was completed, we posted each team's results and created a compilation worksheet.
- The meeting adjourned after the discussion, approximately 12:30pm.

Attached you will find a copy of the compilation worksheet as well as the suggestions made during the discussion part of the workshop.

CC: Carl Winstead

If there are any inaccuracies in these minutes please notify Shannon Coleman @ OBS Landscape Architects within 2 weeks or the contents will be considered accurate.

## COMMUNITY WORKSHOP # 1

<u>CURRENT FACTORS</u>	<u>POSITIVE</u>	<u>NEUTRAL</u>	<u>NEGATIVE</u>
IDENTITY	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
HIGHWAY 70	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
MIX OF LANDUSES / SERVICES	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
SIGNAGE	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
SAFETY (DAY)	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
SAFETY (NIGHT)	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
CIVIC BUILDINGS	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
NCDOT CONTROL OF MAIN ST.	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
TOWN ENTRIES	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
TRAFFIC FLOW	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>

**Downtown Improvements Project**  
Clayton, North Carolina

January 6, 2001




## COMMUNITY WORKSHOP # 1

<u>CURRENT FACTORS</u>	<u>POSITIVE</u>	<u>NEUTRAL</u>	<u>NEGATIVE</u>
HISTORIC BUILDINGS	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
APPEARANCE OF TOWN	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
PEDESTRIAN ACCESS	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
NEW CONSTRUCTION	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
PUBLIC OPEN SPACE	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
PARKING AVAILABILITY	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
PARKING LOCATION	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
VACANT LOTS	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
POPULATION GROWTH	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
TRAIN TRACKS	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>

Downtown Improvements Project  
Clayton, North Carolina

January 6, 2001



## Clayton Downtown Improvements Project Workshop #1 Discussion

### Current Factors

- **Historic Buildings**
- **Appearance of Town**
- **Pedestrian Access**
- **New Construction**
- **Public Openspace**
- **Parking**

### Suggestions

- 1) Room for Improvement
  - 2) Some buildings are an eye sore
  - 3) Find better use for existing buildings.
  - 4) Definitely preserve them
- 1) Clean-up needs improvement
  - 2) We should take pride in our businesses.
  - 3) Add more trees to parking areas
- 1) Crossing streets is dangerous
  - 2) Width of streets is a problem – when cardoors open while parked along streets, delivery trucks stop and take up the lane.
- 1) Good, as long as the design fits the existing fabric of the town
  - 2) Rehab the existing building so they also fit.
- 1) Too much openspace causes lack of continuity along the street.
  - 2) Openspace can cause businesses to be too far apart.
- 1) Parking on Main St. is necessary
  - 2) Shop owner's shouldn't park in front of the shops.
  - 3) Need Off Street Parking
  - 4) Vacant lots are being used as unorganized parking areas, but there is still not enough.
  - 5) Identify parking areas with signage
  - 6) Downtown parking should be paved, and needs additional landscaping – possibly gazebo or natural area in center.

- **Population Growth**
  - 1) Positive if done correctly preventing sprawl and planned for.
- **Train Tracks**
  - 1) Safety at crossings
  - 2) Maintain the land around the tracks – it gets weedy and trashy
  - 3) Additional landscaping – trees – is needed along the railroad right of way.
- **Identity**
  - 1) Need more marketing efforts
  - 2) A lot of people do not know that there is a downtown.
  - 3) Town doesn't really have an identity – i.e. it is not connected to anything specific like antiques, art, farmers market)
- **Highway 70**
  - 1) A lot of people use it
  - 2) Draws business away from downtown
  - 3) Need better signage to get people to go downtown rather than staying on 70.
- **Mix of Landuses / Services**
  - 1) Need more variety of services
  - 2) Lots of businesses exist but need more retail, restaurants, and after 5pm activities.
  - 3) Family owned businesses are important
- **Signage**
  - 1) Need more to get people downtown.
  - 2) Need more around the downtown area.
  - 3) More signs are need along Hwy 70 and NC 42 to direct people to downtown and Main St
  - 4) Possibly rent a billboard advertising Historic Downtown and Main St.
- **Safety**
  - 1) Pedestrian vs. Vehicle is bad
  - 2) Crossing street is tough
  - 3) More crosswalks needed
- **Civic Buildings**
  - 1) Not Adequate now
  - 2) Good that civic buildings are in downtown and not located far away – on Hwy. 70

- **NC DOT Control of Main Street**
  - 1) Town already pays for most of the repairs and upkeep
  - 2) Will DOT reimburse the town what DOT will save by not having to maintain Main St.?
  
- **Town Entries**
  - 1) Definitely need one
  - 2) When you are on Hwy 70, traveling West, the signage is good and the merge off to Main Street is good
  - 3) NC 42 is also heavily traveled and would make a good entry as well.
  
- **Traffic Flow**
  - 1) Late afternoon is tough
  - 2) Pave more of First street
  - 3) Pave Lombard St. north of Main. Possibly connect it to the extension of First St.
  - 4) Improve First St.; complete it from Church St to Lombard. This would allow access to some of the vacant land and allow new construction, while helping traffic flow

Clayton Downtown Improvement Project  
Workshop #1  
Sign-Up Sheet

<u>NAME</u>	<u>ADDRESS</u>	<u>PHONE</u>
8203 Steve Biggs	P.O. Box 879	550-
Dr. Jason Stokes	716 Kershaw Lane	359-9789
Jessica Coutu	P.O. Box 879	553-5002
Cheryl McCardle	101 W. First Street	359-1354
Rick Koobs	80 Austin Pond Rd	550-2694
Larry Kristoff	P.O. Box 495	553-1212
Judy Belvin	P.O. Box 746	553-7188
R. Belvin	(see above)	
Janet Lowry	101 W. First Street	553-2486
Doug McCormac	320 Crescent Dr.	553-6254
Ruby Atchison	304 S. Lombard St.	550-2910
Warren Atchison	(see above)	
Alex Atchison	312 S. Lombard St.	550-2062
Ann Whitley	613 Candlewood Dr.	359-6058
Ron Gurganus	2017 Deer Trail	553-2209
Bob Ahlert	105 Cherry Laurel	550-0845
John Lipscomb	P.O. Box 451	553-3660
Jodi Sager	105 S. Lombard St.	550-0174
Rev. Bill Beyer	124 Falmouth Ct.	553-4784
Mark White	104 Michael Way	553-6627
Paul Parker	P. O. Box 1142	553-4787
Edwina Oliver	105 Edgewater Dr.	553-6515
Barry Woodard		422-4847
James Lipscomb		422-4704
Thad Arunt		553-3945
Butch Lawter		553-4466
Matthew Ansley		833-2350

## Workshop #2



Workshop #2 Presentation

Workshop #2 was a presentation of conceptual ideas from the comments received at the first workshop. Plans were presented which illustrated the following:

- Land Issues
- Development Opportunities
- Pedestrian Connections
- Existing / Future Parking Areas
- Primary Vehicle Routes
- Future Community / Openspace
- Street Improvements
- Street Closures at the Railroad
- Identity / Entry Improvements
- Structures which are contributing

Following the presentations those in attendance were asked to respond and offer input on the conceptual plans.

In general terms the group was in agreement with the direction identified by the conceptual ideas.



Clayton Oil Mills Building

February 6, 2001

## **Town of Clayton Downtown Improvements Project**

### **Community Workshop #2 / Workshop Agenda**

7:00 - 7:15	<p>Review of Workshop #1</p> <p>Purpose of workshop Issues / Factors</p> <p><i>Questions</i></p>
7:15 – 7:45	<p>Further Analysis and Opportunities</p> <p>Entries Connections to rest of Clayton Contributing buildings Non contributing buildings and land uses Land use Open space</p> <p><i>Questions</i></p>
7:45 – 8:00	<p>General Development Concept</p> <p>Opportunities for improvement / development Defining and improving parking areas Streetscape design Intersection improvements Pedestrian connections Land uses</p> <p><i>Questions</i></p>
8:00 – 8:30	<p>Architectural Guidelines</p> <p><i>Questions</i></p>
8:30 – 8:45	<p>Next Step in Process</p>
8:45 – 9:00	<p>Summary / Closing Thoughts / Further Questions</p>

Clayton Downtown Improvement Project  
Workshop #2  
Sign-Up Sheet

<u>NAME</u>	<u>PHONE</u>	<u>ATTENDANCE @ WORKSHOP #1 YES / NO</u>
Doug McCormac	553-6254	YES
Dr. Jason Stokes	359-9789	YES
Jessica Coutu	553-5002	YES
Cheryl McCardle	359-1354	YES
Larry Kristoff	553-1212	YES
Bob Ahlert	550-0845	YES
Kimberly Mack	553-3710	NO
402 Stonebrook Dr. Clayton, NC 27520		
Jerry Thornton	553-2323	NO
Susan LaPresti	550-2600	NO
Amy Whitley	553-5002	YES
Jodi Sager	550-0174	YES
Joyce Blackly	553-6813	NO
Elton Pittman	553-6244	YES
Joe Bunn	553-6244	NO
Lyn Austin	553-4439	NO
Jesse Austin		
Joan Harding	359-2075	NO
Richard Crowder	553-6224	NO
Mary Woodard	553-7549	NO
Barry Woodard	422-4847	YES
James Lipscomb	422-4704	YES
John Barbour	553-7521	NO
Joyce Barbour	553-6523	NO
Butch Lawter	553-4466	YES
Philip Edwards	553-8757	NO

February 26, 2001

## **Town of Clayton Downtown Improvements Project Planning Board Update**

### **Workshop #1**     Saturday, January 6<sup>th</sup>

With Downtown Development Association, community members, downtown merchants, town staff and elected officials we:

- Identified the scope and goals of the study
- Presented an inventory of downtown land uses
- Conducted sessions to gain input regarding issues and concerns
- Prioritized issues

### **Workshop #2**     Tuesday, February 6<sup>th</sup>

Based on the first workshop and the issues raised, we continued with our analysis of downtown and presented conceptual ideas being considered.

These included:

- Development opportunities
- Pedestrian orientation
- Community space
- Realigning Second Street at Main Street

- Architectural design issues

### **Current Activities**

Proceeding with the conceptual ideas, refining them and preparing drawings to illustrate these ideas in a realistic format.

Authoring design guidelines

### **Next Steps**

Council update    3.5.01

Present Draft Master Plan and Report

DDA Meeting     3.20.01

Planning Board   3.26

Town Council     4.2

March 20, 2001

## **Town of Clayton Downtown Improvements Project Progress Memorandum**

### **Downtown Development Association Meeting**

#### **Presentation of Concept Sketches**

Common to the philosophy of our current approach and the recommendations illustrated in the concept sketches are the following goals:

- Identify and portray opportunities for new development
- Improve pedestrian orientation of Main Street
- Identify and define parking areas
- Create a community space
- Establish a sense of entry at key approaches to downtown

#### **Development Opportunities**

Opportunities for new buildings are present along Main Street and Second Street with the removal of incompatible and noncontributing buildings as well as currently vacant lots. From Church Street to past Lombard Street is defined as the center of downtown. It is felt that in this area building heights could go as high as 4 stories and be compatible.

While there should be consistency with regard to the distance from buildings to street new buildings could be set back slightly to allow for wider sidewalks and outdoor dining.

While opportunities along Main Street should serve primarily retail, office, and commercial space, opportunities along Second Street could accommodate residential land use

#### **Pedestrian Orientation**

Pedestrian orientation is improved by confining on street parking to one side along Main Street and the creation of pedestrian corners at street intersection.

By taking control of Main Street from NCDOT the incorporation of special paving can be utilized to draw further attention to the pedestrian.

Pedestrian connections between new buildings from parking areas to Main Street are key components.

#### **Community Space**

The most significant opportunity for a community space is provided by the property across Main Street from the drugstore. Currently used as a parking area half of this property would enable the creation of a central community space while continuing to

provide parking in the short term. In the long term it is suggested that a building could be constructed between the community space and the railroad. This future building should serve some community purpose such as an open market or transit stop.

## **Parking Areas**

Unfortunately the dimensions of Main Street will not allow the introduction of angled parking spaces and continued traffic flow in both directions. To provide some parking on Main Street while improving the pedestrian nature parallel parking is limited to one side.

The concepts illustrate the development of parking areas behind the buildings fronting on Main Street.

Barbour Street is maintained as a one way street. However, the street width is narrowed. Parking areas connect with the street with exit only driveways.

## **Entries**

A major entry from US 70 is defined at the intersection of Main Street and a realigned Second Street. The realignment serves to improve the safety of this intersection while creating the space for the development of an entry feature.

In combination with the historic house, a sequence of walls and plantings create a sense of entry at this intersection. The plantings continue between the street and the railroad continue the sequence towards downtown

## **Current Activities**

Continuation of conceptual ideas, refining them and preparing drawings to illustrate these ideas in a realistic format.

Authoring design guidelines

## **Next Steps**

Planning Board update 3.26

Town Council update 4.2

Present final Master Plan

## **Maintenance Checklist**

Buildings are under constant exposure to the elements of nature and public use. It is therefore important that building owners and tenants take an active role in observing the conditions that are effecting the physical property and participate in an on-going maintenance program to insure the longevity of each building. Most structures are able to withstand some neglect, but the deterioration rate that can occur increases dramatically with time.

The following checklist is meant to be a beginning point to assist property owners and tenants in the evaluation of their property, to identify any maintenance issues, and then to develop an action plan. At the very least the items on this checklist should be investigated every six months.

## A. ROOF

Identify roof type: flat  sloped

Identify roof material: metal  built-up w/gravel  single-ply membrane

Water standing or ponding on roof areas: yes  no

Evidence of past water ponding: yes  no

Any blistering or “bubbles” in roof material: yes  no

Skylight present: yes  no  Water leaks at skylights: yes  no

Parapet flashing and cap conditions: ok  need repair/recaulking

Vent pipe flashing conditions: ok  need repair/recaulking

Flashing conditions / rooftop mechanical equipment: ok  needs repair/recaulking

Chimney covers in place: yes  no

*Note: Limit amount of walking on roof areas as this can cause damage to the roof resulting in water leaks.*

## B. GUTTERS AND DOWNSPOUTS

Existing gutters: yes  no

Clean and clear of any material: yes  no

Joints in gutter watertight: yes  no

Existing downspouts: yes  no

Downspouts open and cleaned: yes  no

Joints in downspouts watertight: yes  no

Existing scuppers: yes  no

Scuppers clean and clear of any material:    yes     no

Any rusting of gutters, downspouts, or scuppers:    yes     no

Signs of water on building exterior walls adjacent downspouts:    yes  no

**Caution:** *Water is one of the single most destructive forces that can act on a building. Careful attention must be paid to ensure no roof leaks, and that water is being properly carried away from the building with gutters and downspouts that are clean. Water infiltration into a structure can result in damages that increase exponentially with time if left unattended.*

## C. EXTERIOR WALLS

Exterior wall material types:    brick     stone     concrete     stucco     wood     metal

Exterior cracks visible:    yes     no

Previously repaired cracks:    yes     no  Condition: \_\_\_\_\_

Exterior wall surface clean:    yes     no

Loose, broken, or missing brick:    yes     no

Condition of mortar joints:    acceptable     need repointing

Existing exterior wall surface painted:    yes     no  Condition: \_\_\_\_\_

**Note:** *The cleaning, repairing, and repointing of masonry surfaces is a specialized process and should never involve high pressure water (power washing) or sandblasting. Cleaning should be chemical and with low pressure water. Repointing of mortar should begin with an analysis of the existing mortar with new mortar matching the color, joint type, texture, and most importantly, the strength.*

## D. WINDOWS AND DOORS

Original windows:  Replacement windows:

Original doors:  Replacement doors:

Existing windows operable:    yes     no

Window material: wood  vinyl  aluminum

Finish: painted  clad  natural metal

Broken panes: yes  no

Operable hardware: yes  no

Wood areas fully covered with paint: yes  no

Visible damaged or deteriorated wood: yes  no

Window sill material: wood  masonry  stone  metal

Exterior jambs/heads/sills properly caulked: yes  no

Exterior sill condition: acceptable  need of repainting/repair

Visible signs of interior water intrusion around windows: yes  no

Existing door material: wood  metal  aluminum

Broken glass: yes  no

Existing door hardware operational: yes  no

Existing doors properly weather-stripped: yes  no

Wood areas fully covered with paint: yes  no

Thresholds in place: yes  no

## **E. STRUCTURE**

New interior wall cracks: yes  no

Visible change in existing wall cracks: yes  no

Noticeable changes in floor levels: yes  no

Any active water damage to roof structure: yes  no

Any wood structure in contact with soil: yes  no

Recent termite protection: yes  no

Vapor barrier installed over any crawl space areas: yes  no

## **F. SYSTEMS**

System type: gas  oil  electric

Air conditioning: yes  no

Service contract current for mechanical systems: yes  no

Filters changed: yes  no

Air returns unobstructed: yes  no

Any additional electrical loads added: yes  no

Any plumbing leaks: yes  no

## **Additional Resources**

Town of Clayton  
Director of Planning  
P.O. Box 879  
Clayton, NC 27520

Clayton Downtown Development Assoc.  
c/o Town of Clayton  
P.O. Box 879  
Clayton, NC 27520  
Mr. Barry Woodard -Current President

Town of Clayton Building Inspections  
P.O. Box 879  
Clayton, NC 27520  
Phone (919) 553-1545  
Fax (919) 553-1541  
Mr. Dan Schwoeble

United States Department of the Interior  
National Park Service  
Preservation Assistance Division  
PO Box 37127  
Washington, DC 20013-7127  
(202) 343-9578

United States Department of the Interior  
National Park Service  
Southeast Regional Office  
75 Spring Street SW  
Atlanta, GA 30303  
(404) 331-4998

North Carolina Department of Cultural Resources  
North Carolina Archives and History  
State Historic Preservation Office  
515 N. Blount Street  
Raleigh, NC 27605  
(919) 733-6547

North Carolina Department of Insurance  
Engineering and Building Code  
410 N. Boylan Avenue  
Raleigh, NC 27603  
(919) 733-3901

North Carolina State University  
College of Design Library  
Brooks Hall  
North Carolina State University  
Raleigh, NC 27695  
(919) 515-2207

National Trust for Historic Preservation  
1785 Massachusetts Avenue, NW  
Washington, DC 20036  
(202) 673-4000

National Trust for Historic Preservation  
Southern Regional Office  
456 King Street  
Charleston, SC 29403  
(803) 722-8852  
Mr. John Hildreath

North Carolina Main Street Program  
1307 Glenwood Avenue  
Suite 250  
Raleigh, NC 27605  
(919) 733-2850  
Mr. Rodney Swink

North Carolina Downtown Development  
Association  
P.O. Box 340  
Lexington, NC 27293-0340  
Phone: (336) 249-0383  
Email: [uptownlex@lexconinc.net](mailto:uptownlex@lexconinc.net)  
Ms. Liz Parham

## **North Carolina Division of Archives and History State Historic Preservation Office**

### **Federal Preservation Tax Credits for Historic Building**

#### *What is the Federal Historic Preservation Tax Credit?*

An incentive to taxpayers who contribute to the preservation of historic buildings by rehabilitating them.

Provides a dollar-for-dollar reduction of federal income tax owed equal to 20 percent of the cost of rehabilitating “certified historic structures.”

Project must go through the three-part “Historic Preservation Certification Application” process at the state and federal levels - the State Historic Preservation Office and the National Park Service.

#### *Who may apply for the tax credit?*

Owners of potential “certified historic structures.”

Lessees of potential “certified historic structures” with a lease term of 27.5 years of residential property and 39 years for nonresidential property.

#### *What buildings are eligible?*

Buildings listed individually in the National Register of Historic Places or potentially eligible to be listed. Buildings not listed are required to be listed as part of the final certification.

Buildings located in a “registered historic district” or a potentially eligible district that contribute to the significance of the district. Districts not listed are required to be listed as part of the final certification.

Must be a “certified historic structure” as determined by the National Park Service through the “Historic Preservation Certification Application Part 1 - Evaluation of Significance.”

Must be used in a trade or business or held for the production of income for either rental residential or nonresidential purposes.

#### *What rehabilitations qualify?*

Must be substantially rehabilitated - expenditures must exceed the greater, the “adjusted basis of the building or \$5,000, within 24 months or 60 months for phased projects.

Rehabilitation must be consistent with the historic character of the building and where applicable, with the district in which the building is located as determined by the National Park Service through the “Historic Preservation Certification Application Part 2 - Description of Rehabilitation.”

Must meet the Secretary of the Interior’s “Standards for Rehabilitation.”

*What rehabilitation expenditures qualify?*

Expenditures incurred in connection with the rehabilitation of a “certified historic structure” that are properly chargeable to a capital account.

Includes: rehabilitation cost; construction interest and taxes; architectural and engineering fees; legal and professional fees; developer’s fees; and general and administrative cost.

Does not include: acquisition cost; enlargement cost; acquisition interest and taxes; realtor’s fees; paving and landscaping cost; sales and marketing costs; or new building construction cost.

*When can a taxpayer claim the credit?*

Generally, for the taxable year in which the rehabilitated building is placed in the service. If all of the credit cannot be used, the excess can be carried back three years and then forward 15 years.

For phased projects, before completion for the entire project on the basis of “qualified progress expenditures” if construction is planned for two or more years.

Must be a “certified rehabilitation” as determined by the National Park Service through the “Historic Preservation Certification Application - Request for Certification of Completed Work” - a copy of this certified application must be filed with the tax return claiming the tax credit.

*When are the tax credits subject to recapture?*

None if a qualified rehabilitated building is held by the taxpayer for longer than five years after the building is placed in service.

One hundred percent is recaptured if the owner dispose of the property within one year and the amount of such recapture is reduced by 20 percent for each full year that elapses thereafter.

*What are the fees for processing rehabilitation certification requests?*

The National Park Service’s fee for review of rehabilitation projects ranges from \$500 to \$2,500 for projects with rehabilitation construction costs ranging from \$20,000 to \$1,000,000 plus.

*What is the 10 percent federal rehabilitation tax credit?*

Available for non-historic buildings built before 1936 used for nonresidential purposes. No “Historic Preservation Certification Application” required.

*How does the State of North Carolina Historic Preservation Tax Credit work?*

Effective for taxable years beginning on or after January 1, 1994, taxpayers who receive the 20% Federal Historic Preservation Tax Credit will also qualify for an additional 5% tax credit from the State of North Carolina.

This information sheet describes the Federal Historic Preservation Tax Credit program in very general terms only. Taxpayers should consult an accountant, tax attorney, or other professional tax advisor, legal counsel, or the Internal Revenue Service for help in determining the tax and other financial implications of any matter discussed here. For further information regarding the National Park Service regulations governing the procedures for obtaining historic preservation certifications, contact Tim E. Simmons, AIA, Preservation Tax Credit Coordinator, Restoration Branch, State Historic Preservation Office, North Carolina Division of Archives and History, 109 Jones Street, Raleigh, North Carolina 27601-2807; 919-733-6547.

## **The Secretary of the Interior's Standards for Rehabilitation**

*The following Standards are to be applied to specific rehabilitation projects in a reasonable manner, taking into consideration economic and technical feasibility.*

1. A property shall be used for its historic purpose or be placed in a new use that requires minimal changes to the defining characteristics on the building and its site and environment.
2. The historic character of a property shall be retained and preserved. The removal of historic materials or alterations of features and spaces that characterize a property shall be avoided.
3. Each property shall be recognized as a physical record of its time, place, and use. Changes that create a false sense of historical development, such as adding conjectural features or architectural elements from other buildings, shall not be undertaken.
4. Most properties change over time; these changes that have acquired historical significance in their own right shall be retained and preserved.
5. Distinctive features, finishes, and construction techniques or examples of craftsmanship that characterize a historic property shall be preserved.
6. Deteriorated historic features shall be repaired rather than replaced. Where the severity of deterioration requires replacement of a distinctive feature, the new feature shall match the old in design, color, texture, and other visual qualities and, where possible, materials. Replacement of missing features shall be substantiated by documentary, physical, or pictorial evidence.

## Downtown Clayton: Vision 2012

*(adopted 6/18/07)*

The new streetscape of Main Street thrives amid a profusion of flowers and plantings, decorative street-crossings, walkways, and interesting buildings. Downtown is well-lit and safe. Overall, Clayton has a clean, uncluttered look, especially since the utility lines were put underground.

Now a diverse business center, Clayton retains its small town charm as it steams toward its progressive future. Trendy restaurants offer sidewalk cafes and entertainment for all tastes. Every storefront is occupied and restored to its original architectural glory accented by attractive landscaping. A variety of shops are open evenings to accommodate the needs of residents and visitors alike. The open-air market provides fresh, local produce and crafts.

Clayton's growing visual and performing arts community is alive with artists and music. Public art throughout downtown provides beauty and interest. The Clayton Center continues to flourish with live professional and community performances.

Attractive and informative signs welcome residents and visitors to downtown shops and parking. They guide the way to downtown from other Clayton communities.

The visitor's center provides us with maps of historic Clayton and a schedule of events, such as the Harvest Festival and Millstock, for the downtown area. The Clayton History Museum offers an interactive map and walking tour of the historical area and other points of interest.

Downtown has truly become the social and cultural hub of Clayton.



Edward Knight  
President

Richard Rairigh  
Secretary

Betsy Grannis  
Vice Chair  
Organization

Debbie Hammond  
Vice Chair  
Promotion

Leslie Hubbard  
Vice Chair  
Economic  
Restructuring

Boo Carver  
Vice Chair  
Design

Debie  
Austermuehle

Jeme Avent

Margaret Lee

Michael Sims

*Ex Officio*

Michael Grannis,  
Town Council

Bruce Naegelen,  
Downtown  
Development  
Coordinator

Skip Browder,  
Planning Director

Steve Biggs,  
Town Manager

## Clayton Downtown Development Association, Inc.

*Vision 2012: Downtown is the social and cultural hub of Clayton*

North Carolina Main Street Community

TO: Downtown Stakeholders  
FROM: Boo Carver, Design Cmte Chair  
DATE: May 6, 2010  
SUBJECT: Invitation to Workshop – Downtown Master Plan Update

In 2001, the Town of Clayton developed a downtown master plan entitled, "Downtown Improvement Projects." It is time for that document to be updated and **your input is necessary for a viable downtown plan for the next 10 years.**

As a group, we will review the existing plan and identify issues and concerns that have evolved over the past 10 years, such as historic buildings, appearance, pedestrian access, new construction, public space, parking, growth, railroad, identity, US 70, mix of land uses/services, signage, safety, civic buildings, NCDOT Control of Main Street, entryways to downtown, and traffic flow.

The 2001 Downtown Master Plan is available for review on the Town's website at [www.downtownclaytonnc.org/Business/Downtown.aspx](http://www.downtownclaytonnc.org/Business/Downtown.aspx)

Thursday, May 20, 2010  
10:00 am – 1:00 pm  
Rotary Room at The Clayton Center

**Lunch and refreshment will be provided, so please let us know by May 18 that you will be able to participate.** Contact the Downtown Development Office at 553-1545 or [bnaegelen@townofclaytonnc.org](mailto:bnaegelen@townofclaytonnc.org).

If you are unable to participate on May 20, we hope you will review the 2001 Plan online and provide written comments by May 18 to help with the discussion in your absence.

A follow-up workshop is scheduled from 6:30 pm – 8:30 pm for Thursday, June 3 to seek additional comments from those of you who cannot attend on the 20<sup>th</sup> and the general public.

**The outcome of this effort will be a plan that will guide downtown development efforts for the next decade.**

Thank you for your involvement!

Town of Clayton  
Downtown Master Plan Update  
Workshop #1

May 20, 2010

AGENDA

10:00 – 10:15 Background & Introductions

Purpose of Workshop

Tools

- DDA Plan of Work 2010 - 2012
- Historic District
- Parking Needs Assessment & Inventory
- Vision Statement
- NC Main Street Resource Team Report

10:15 – 11:00 Group Discussion Activity

11:00 – 11:15 Break & Bring Lunch to Table

11:15 Share Activity Results

11:30 Review Master Plan Illustrations

- Identify issues
- Discuss potential solutions

12:50 Next Steps  
Future Meetings  
Closing Remarks

Town of Clayton  
Downtown Master Plan Update  
Workshop #2

June 3, 2010

AGENDA

- 6:00 – 6:15      Background & Introductions
- Purpose of Workshop
- Tools
- DDA Plan of Work 2010 - 2012
  - Historic District
  - Parking Needs Assessment & Inventory
  - Vision Statement
  - NC Main Street Resource Team Report
- 6:15 – 6:30      Group Discussion Activity
- 6:30                Share Activity Results
- 6:45                Review Master Plan Illustrations
- Identify issues
  - Discuss potential solutions
- 8:15                Next Steps  
                         Closing Remarks

### Downtown Master Plan Update Workshop # 2

Please rate each of the topics below, as they currently exist in Clayton, as a positive, neutral or negative aspect of downtown Clayton.

<u>CURRENT FACTORS</u>	<u>POSITIVE</u>	<u>NEUTRAL</u>	<u>NEGATIVE</u>
------------------------	-----------------	----------------	-----------------

Historic Buildings	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
--------------------	--------------------------	--------------------------	--------------------------

---



---



---

Appearance of Town	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
--------------------	--------------------------	--------------------------	--------------------------

---



---



---

Pedestrian Access	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
-------------------	--------------------------	--------------------------	--------------------------

---



---



---

New Construction	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
------------------	--------------------------	--------------------------	--------------------------

---



---



---

Public Open Space



---

---

---

Parking Availability



---

---

---

Parking Location



---

---

---

Vacant Lots



---

---

---

Population Growth



---

---

---

Train Tracks



---

---

---

Identity



---

---

---

Highway 70



---

---

---

Mix of Land Uses/Services



---

---

---

Signage



---

---

---



## Downtown Master Plan Update Workshop

May 20 & June 3, 2010

### 2010 CURRENT FACTORS

#### HISTORIC BUILDINGS

Positive 111  
Neutral 111  
Negative

- Façade grant program an important tool
- Education program
- Attract new uses/vitality
- Recent restoration efforts - good
- More info and need to highlight these buildings – new folks don't know which buildings are historic
- Recognized, retained and developing a historic district
- Not much of a driving point (attraction)

#### APPEARANCE OF TOWN

Positive 11  
Neutral ½ 1  
Negative ½ 11

- Lacks some charm "feel" – too much sameness
- Marketing to attract new
- Garden club plants/flowers
- Improved coordination of "fixtures"
- South entrance bleak
- Streetscape – need more shade trees, - places to sit/hangout
- Wayfinding signs fitting downtown character
- Better sidewalk appearance
- Some improvements seen in last few years
- Signage issues
- Trees growing out of rooftops and trash in street
- Need more code enforcement
- Lacks charm, color (flowers)
- Main Street parking is crowded (one side only)
- Business vehicles on dirt parking lot

- Pleasing, appearance of lots of activity
- Work on sidewalks - cleanup

#### PEDESTRIAN ACCESS

Positive 11  
Neutral 1  
Negative 111

- Increase reasons to walk
- Evaluate and improve safety
- Sign control
- Improve appearance & number of crosswalks
- Sidewalks across tracks – (Access to DT)
- Wider sidewalks (more activity)
- Crosswalks – brick, etc)
- Greenway – downtown connection
- Need connection to areas outside of town
- More connections to different areas of town
- Sidewalks need improvement
- Connecting access to and between parks
- Sidewalks need updating for handicapped access and from other side of railroad tracks
- Sidewalks end in certain areas of downtown neighborhoods

#### NEW CONSTRUCTION

Positive 11  
Neutral 111  
Negative

- Must blend and match – guidelines
- Mixed uses
- Attracting developers that "match vision" of town
- Housing – multi-family, 2<sup>nd</sup> story apts on top of shops
- Art – 2<sup>nd</sup> story as art studios
- Need more incentives to attract business
- Too many vacancies
- Work with what is here already
- Need more of it
- Consistent building pattern of commercial buildings

Downtown Master Plan Update Workshop  
Issues/Areas of Concern

**LA-1**

ISSUE	AREA OF CONCERN	COMMENT
Driving	South on N Church Street	low visibility due to incline at RR
Environment	Obstacles	Environmental issues obstacle to plan
Parking	Church Street (Main to Second)	needs handicapped parking by Woman's Club
Parking	Church/Second/Main/Fayetteville	Need parking areas
Parking	Church/Second/Main/Fayetteville	Suggestion: vacant lot at SE corner of Horne/Fayetteville
Parking	Church/Second/Main/Fayetteville	Suggestion: Make Church St one-way
Parking	Church/Second/Main/Fayetteville	Remove buildings west of Library
Parking	N Church/First Street	Cars park on sidewalk - maybe reduce sidewalk width
Parking	Town Square	Utilize vacant lot at corner of O'Neil & Main
Sidewalk	Ellington Street	no sidewalk
Sidewalk	Front Street to Main Street	no pedestrian passageways
Sidewalk	all downtown	Sidewalk inventory suggested
Sidewalk	Church Street to Front Street	
Sidewalks	North of Main	Continuous sidewalks
Street wall	New buildings not at street	Plan needs to be enforced

**LA-2**

ADD	Incorporate Food Town into plan	underutilized space/multi-use/multilevel bldgs
Appearance	Smith Street	Closure has hard barrier - can we make it more attractive?
Greenway		accesses to downtown?
Parking	North & South of Main St	formalized parking behind buildings
Parking	Behind Mulberry & Elmore Furniture	Better utilization of parking
Ped/Vehicular Underpass	Main Street to Front Street	
Public Transportation	to connect	
Sidewalks	North side to Main Street	no sidewalks
Sidewalks	Front Street from PO to Church St	Is this part of the bond project?

**LA-3**

Labeling	Compton House	vs Durham House and location of bldg
----------	---------------	--------------------------------------

Public Transportation	Town Lot and west	Commuter rail development
Road	Second Street to Central St	Keep concept of alignment
Road	Second Street to Central St	Keep concept of alignment
<b>LA-4</b>		
Bike Parking	Downtown	need racks
Pedestrian	US 70	pedestrian access across US 70
Public transportation	Commuter Stop in downtown	Need to push
Road	Main St / US 70 Alignment	roadway changed in 2008
Road	Main St / US 70 Alignment	update alignment
Sidewalk Plan	US70 - Downtown	
Sidewalks	US 70 to Stallings Street (RR Overpass)	add
Wayfinding	US 70 / Main Street/Fayetteville St	Signage

## GENERAL COMMENTS

OTHER	Meet with Town Mgr/Mayor to discuss issues
OTHER	Prioritize Issues
OTHER	MSD/Tax District
OTHER	Under-performing assets
OTHER	People don't care about downtown because they don't come downtown
OTHER	Figure out how to get "them" involved
OTHER	Create something to bring them downtown